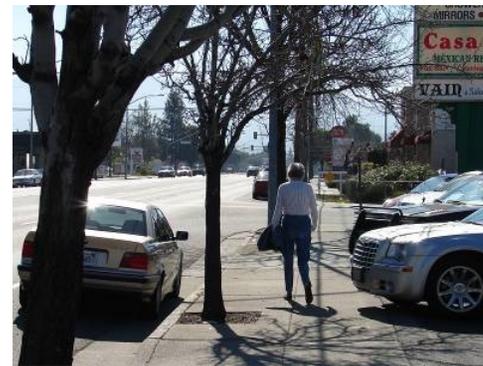


WINCHESTER BOULEVARD MASTER PLAN

CITY OF CAMPBELL

City Council Approval of Master Plan - February 17, 2009



*Prepared by the City of Campbell
with assistance from
Bottomley Associates Urban Design & City Planning*

WINCHESTER BOULEVARD MASTER PLAN

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I. Background & Overview

Master Plan Process

In June of 2007, the Planning Commission reviewed proposed revisions to the development standards and design guidelines for Winchester Boulevard from the northern City limits to Budd Avenue. The guidelines were prepared in response to an increasing number of developments and project applications for properties along the corridor. An initial community input meeting was held in August 2007, and in response it was recommended that the effort to revise the development standards and design guidelines be expanded to a more comprehensive master planning process that would address the following elements:

- *Incorporation of public works improvements in guidelines and standards for private development*
- *Establishing an overall boulevard vision concept that combines private infill development and street improvements*
- *Improving pedestrian accessibility and creating a more attractive pedestrian environment throughout the corridor*
- *Providing for undergrounding of overhead utilities in accordance with a future Underground Utility District*
- *Creation of a safer and more attractive connection between the Community Center and Downtown Campbell*

- *Refinement of the initial design guidelines and recommendations per community, Planning Commission, and City Council comments*

In November 2007, the City Council directed City staff and consultants to prepare a *Winchester Boulevard Master Plan* document containing policies that addressed these elements. As part of the master plan process, additional public outreach and study sessions with the Planning Commission and City Council were conducted during the spring of 2008 through early 2009.

On April 24, 2008, a community meeting was held to obtain feedback on issues and concerns regarding the Winchester corridor and public expectations for the content of a master plan. A follow-up meeting was held on May 29, 2008, with a presentation that detailed new options for private development standards and public improvements in the area while addressing some of the public's concerns and desires for the area. The main concerns included traffic congestion, parking, building height, side street/neighborhood impacts and the types of new businesses that could come into the project area.

Following the community meetings, a study session was held with the Planning Commission on June 24, 2008. During this meeting, staff and consultants reviewed the May 29th presentation and requested the Commission's input. The Commission noted appreciation for the addition of streetscape improvements and also stated the plan should consider east/west neighborhood context, the character of surrounding neighborhoods, side street traffic patterns, a balance in the scale of new development and access, circulation and delivery impacts.

A subsequent study session was held with the Planning Commission on August 12, 2008, where additional information was provided regarding the envelope of building height and lot depth, setbacks, circulation, parking and building form and architectural guidelines. A Council Study Session was held on September 16, 2008, to review the initial draft Master Plan recommendations.

Public hearings were then held at the October 28, 2008 and January 27, 2009 Planning Commission Meetings. The final plan was adopted by the City Council following the February 17, 2009 City Council Public Hearing.

Public meeting notices were sent to residents, property owners, the Campbell Chamber of Commerce, and the Downtown Campbell Business Association. Information on the project was also posted on the City's web site.

Summary Master Plan Recommendations

The goal of the Master Plan is to transform this segment of Winchester Boulevard into a vibrant mixed-use, pedestrian-oriented street, lined with ground-level businesses with residential or office above. This plan will provide a physical and visual connection to Downtown and Campbell Avenue, as well as help increase Central Campbell's walkability and livability by providing increased housing and shopping opportunities

Development Policies Summary

1 - The Master Plan establishes three policy subareas, reflecting existing patterns of parcel size, configuration, and street /frontage

condition. "Area 1" extends from Camden Avenue on the south to Budd Avenue; "Area 2" extends from Budd Avenue to Campbell Avenue; "Area 3" extends from Campbell Avenue north to Rosemary Lane.

2 - First floor office, residential, and/or commercial space is recommended for the areas south of Budd and north of Campbell Avenue; i.e., Areas 1 and 3, respectively. New development in the area south of Budd should reflect proximity to the Winchester transit center.

3 - First floor commercial space is recommended along the street frontage between Budd and Campbell Avenue in Area 2, with residential or office space above. New development in this area should complement Downtown with an emphasis on small-scale, pedestrian-oriented commercial uses, including specialty retail, restaurants, and local/neighborhood services.

4 - Building heights should be a maximum of 45 feet in Area 2 and 55 feet in Areas 1 and 3. Building heights should be related to lot depth, and should step down adjacent to single-family residential areas. Variation in building heights and variation of building rooflines and massing is strongly recommended.

5 - Contribution(s) to streetscape-related capital improvements should be considered for new development. These improvements could include widened sidewalks, corner curb bulb-outs, street trees, median islands, and/or other elements.

6 - Parking should be located to the rear, side, or below buildings, and strongly discouraged along the Winchester Boulevard frontage.

7 - Special circulation and parking reviews will be performed for development applications in all areas to address potential neighborhood impacts.

Public Improvements Summary

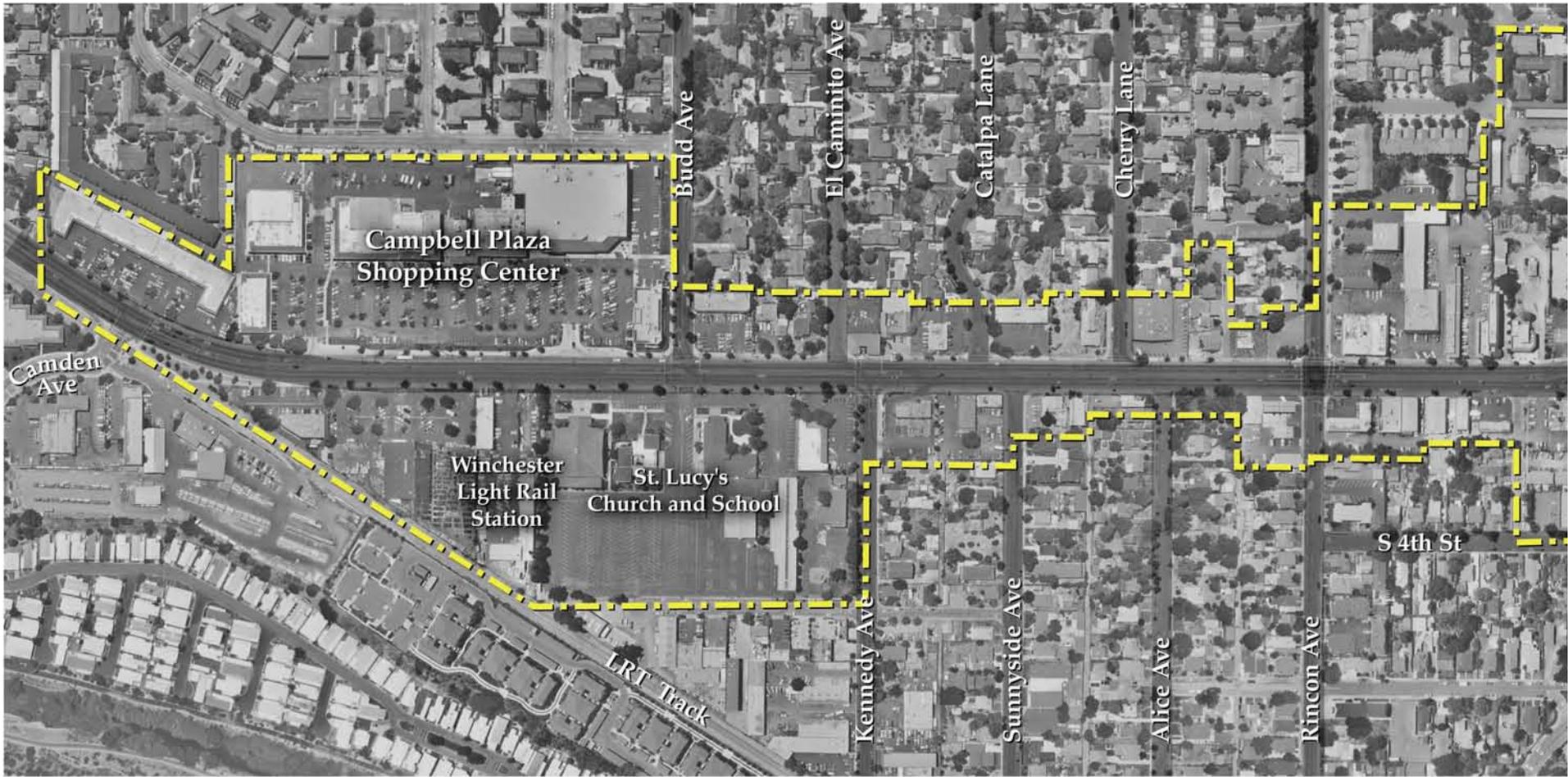
1 - The Master Plan proposes public improvements that complement the three policy subareas, with pedestrian-oriented improvements focused on Area 2, located between Budd and Campbell Avenue.

2 - The City will evaluate the feasibility of full bulb-outs at all signalized pedestrian crossings, and partial bulb-outs at non-signalized locations as appropriate to improve pedestrian circulation along the corridor.

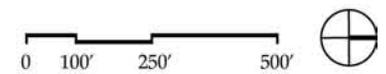
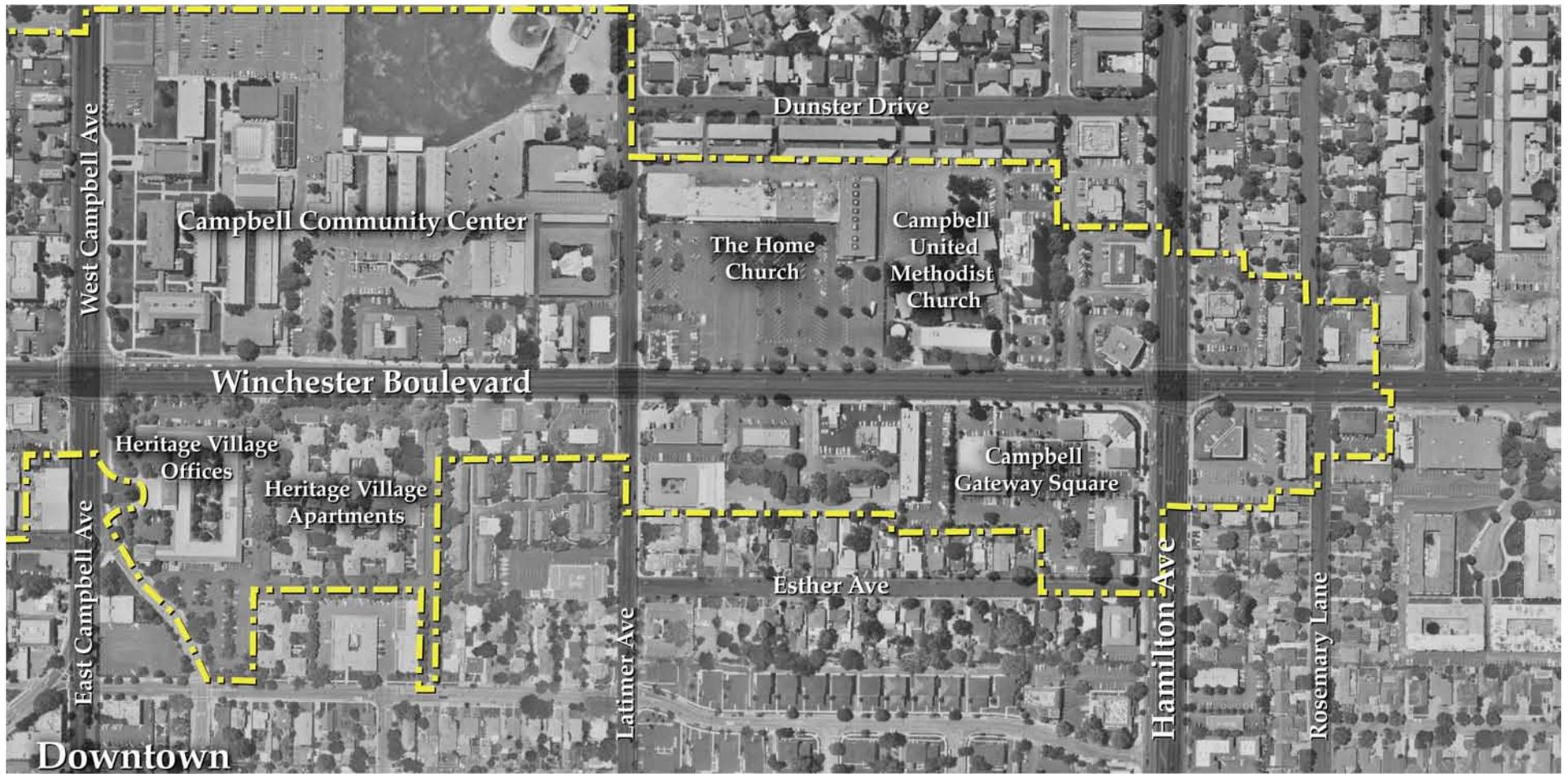
3 - Pedestrian-oriented streetlights should be installed throughout Area 2 consistent with the City's planned utility undergrounding effort.

4 - Street trees should be installed along the frontage in all *Master Plan* subareas. A curbside planting strip is recommended in Areas 1 and 3 where no on-street parking exists or is proposed, and tree grates should be installed in Area 2.

5 - A landscaped median island with left turn pockets is recommended for Areas 1 and 3. This island would be located within existing striped median areas, and could be installed without reducing existing travel lanes. The design and layout of the median islands shall include a review and evaluation of entry locations to parcels along the corridor and potential U-turn pockets.



Winchester Boulevard Plan Area Map – South of Campbell Avenue



Winchester Boulevard Plan Area Map – North of Campbell Avenue

II. Existing Conditions

The Master Plan Area extends from Rosemary Lane on the north to Camden Avenue on the south, a distance of approximately 1.3 miles. It also extends along West Campbell Avenue approximately 800 feet west to Jeffers Way. The plan area incorporates the Winchester Boulevard roadway as well adjacent frontage properties. The “Plan Area Map” on the preceding pages indicates Master Plan Area boundaries and highlights key properties. A portion of the area including the Community center is within the boundaries of the City’s Redevelopment Area.

Existing Development

South Winchester Boulevard is a major arterial that serves as a gateway into the City of Campbell and is the primary north/south thoroughfare for the central portion of the city. Currently, it is lined by a variety of older commercial and service-oriented businesses, and is ripe for re-development.

The General Plan designates the *Winchester Boulevard Master Plan* area primarily as Central Commercial. The majority of Master Plan area parcels are zoned Planned Development, with either a Central or General Commercial General Plan Land Use Designation. Some parcels are also located within the Campbell Redevelopment Area.

The size of Master Plan Area parcels and the potential for new development and/or major renovations are two important factors in anticipating new development and in preparing development and design guidelines. These factors are described below.

Parcel Size. Parcel sizes vary dramatically throughout the corridor, with smaller properties concentrated between Budd and Campbell Avenues. Properties in this area are as small as 0.1 acres, with approximately 0.4 acres a typical size. North of Campbell and south

of Budd Avenues parcels are significantly larger. The Campbell Community Center and the Campbell Plaza shopping center are the largest parcels in these areas, with sizes of approximately 11 acres and 28 acres, respectively. The “Parcel Size Diagram” on the following page illustrates patterns of parcel size within the Master Plan Area.

Potential for Change. The “potential for change” for Master Plan area properties depends on the likelihood that significant new investment in private development will occur. As illustrated by the “Potential for Change Diagram” on the following page, this could range from very low for properties that have been recently developed or are publicly owned, to very high for properties that are vacant, for sale, or have pending development plans.

As the diagram illustrates, most of the largest properties in the Master Plan Area, including Campbell Plaza, St. Lucy’s Church and School, and the Campbell Community Center have low potential for change. By comparison, the site of the recently approved “Merrill Gardens” mixed-use senior housing project adjacent to Rincon Avenue has a high potential for change.

An important and related factor is the number of small parcels developed with buildings that have a relatively high floor-area-ratio (FAR); i.e., high amounts of building floor space relative to site area. These parcels were typically developed prior to establishment of the City’s current parking standards and are therefore “underparked” (undersupplied with on-site parking spaces). These parcels are indicated as being in the low potential range because they have a relative advantage over vacant property for which parking must be provided as part of new development.

Roadway and Frontage Conditions

Winchester Boulevard is a four-lane roadway with a continuous center left turn lane/painted median. Average daily traffic within the Master Plan area is approximately 28,000 vehicles, with north-south traffic roughly balanced. Signalized intersections in the master plan area are located at Camden Avenue, the entrance drive to the Winchester LRT Station, Budd Avenue, Kennedy Avenue, Rincon Avenue, Campbell Avenue, Latimer Avenue, and Hamilton Avenue. Campbell Avenue and Hamilton Avenue are major east-west arterial streets.

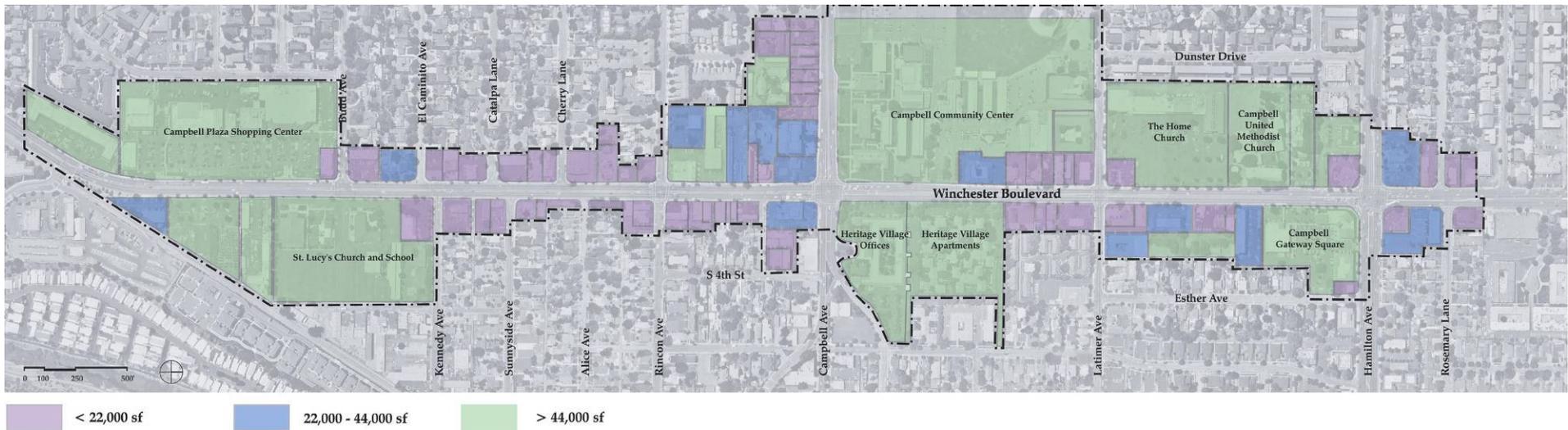
The street right-of-way is approximately 90 feet in width, with a street curb-to-curb dimension of approximately 70 feet. Curbside parking is available along the easterly street frontage between Camden Avenue and Campbell Avenue, and along the westerly frontage from Budd Avenue to Campbell Avenue. There is limited curbside parking on the west side of Winchester Boulevard north of Campbell Avenue, in front of Campbell United Methodist Church and the Campbell Community Center. No curbside parking is available along the east side of Winchester Boulevard north of Campbell Avenue, or along the west frontage south of Budd Avenue. Winchester Boulevard is a "Bike Route" but does not have striped bike lanes.

Block lengths along Winchester Boulevard vary from subarea to subarea. North of Campbell Avenue and south of Budd Avenue blocks are quite large; e.g. there is only one cross street between Campbell and Hamilton Avenues, making blocks in that area approximately 1,250 feet in length. By contrast, blocks in the area between Rincon and Budd Avenues are typically from 300 to 325 feet in length. Blocks in this area are also offset, creating some awkward and conflicting vehicle turning movements to, from, and crossing Winchester Boulevard. Offset blocks and a limited number of signalized intersections tend to limit the number of pedestrian crossings in this area.

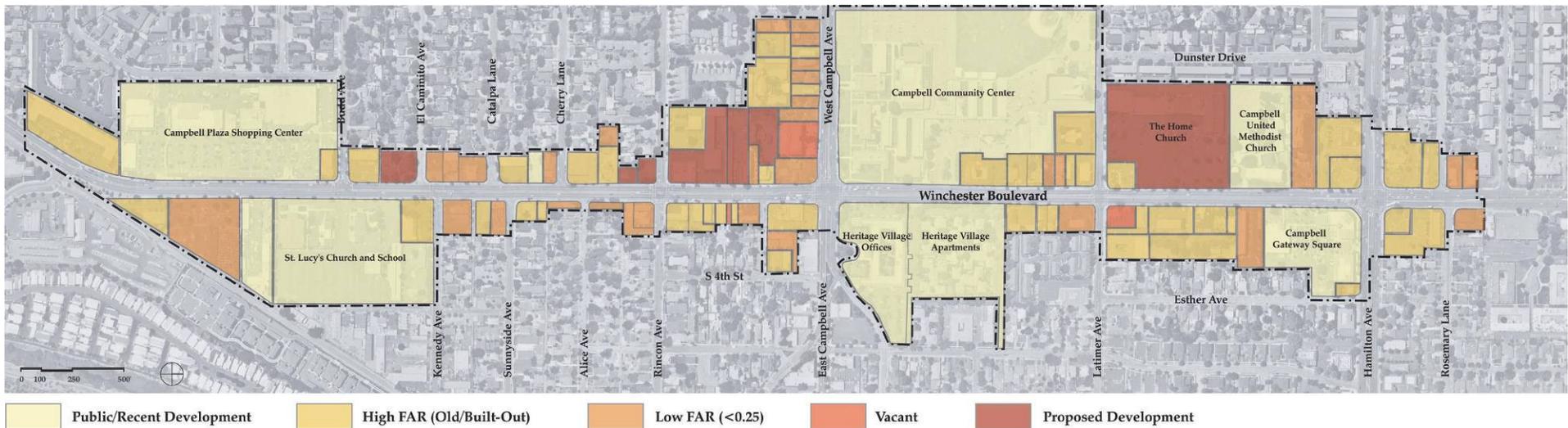
Sidewalk conditions are relatively consistent, with walks approximately 10 feet in width. In a number of locations adjacent commercial buildings are set back with additional paved or landscaped areas of 5 feet or more, giving the sidewalks a more spacious feeling. In other locations surface parking areas abut the sidewalks directly.

Street trees are scattered along the frontages, with most blocks lacking them entirely. The corridor's largest and most attractive street trees are located along the Heritage Village Apartments complex, just north of Campbell Avenue. Smaller, more recently planted street trees are located along the easterly frontage south of Kennedy Avenue, and along the westerly frontage near Latimer Avenue. "Cobra-head" highway-type street lights line the street, with an occasional mix of luminaires mounted to existing wood utility poles. There are no benches or other street furnishings.

The current configuration of the Winchester/Campbell Avenue intersection tends to deter pedestrian movement between the area's two major destinations, the Campbell Community Center and Downtown. Large-radius turns to and from Winchester Boulevard encourage traffic speeds higher than desirable for a pedestrian route, and at 70 feet, the Winchester crossing distance can be intimidating. In addition, the East Campbell Avenue lane split at South 4th Street and the entrance island at Heritage Village combine to make the pedestrian route between the two destinations circuitous and hard to discern.



Parcel Size Diagram



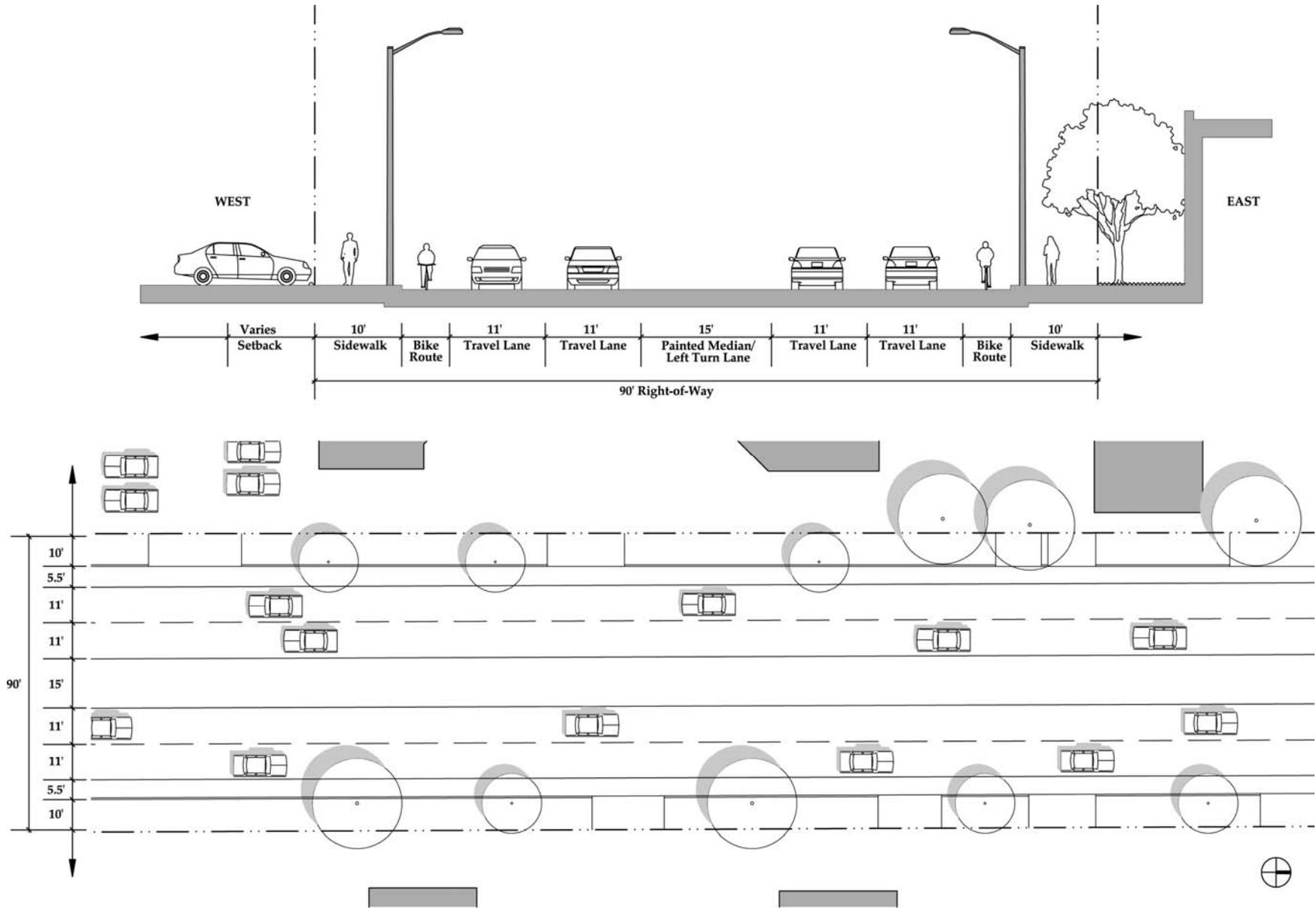
Potential for Change Diagram

Notable Master Plan Area Features

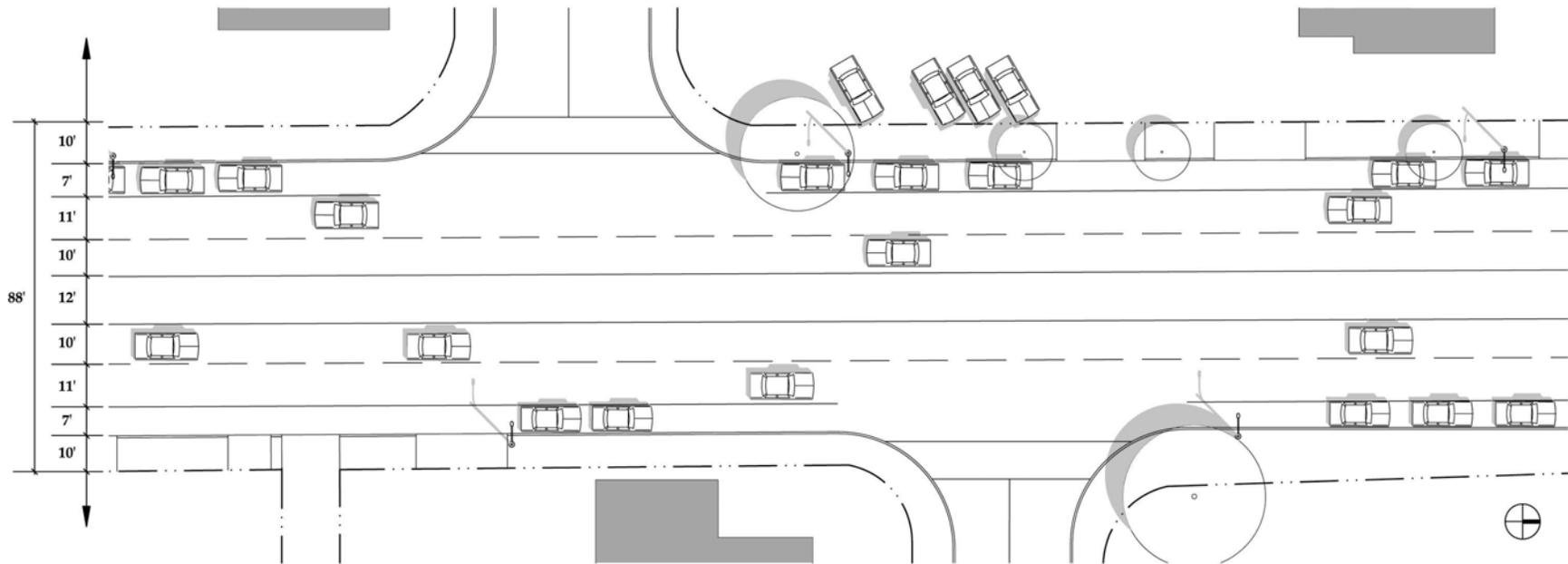
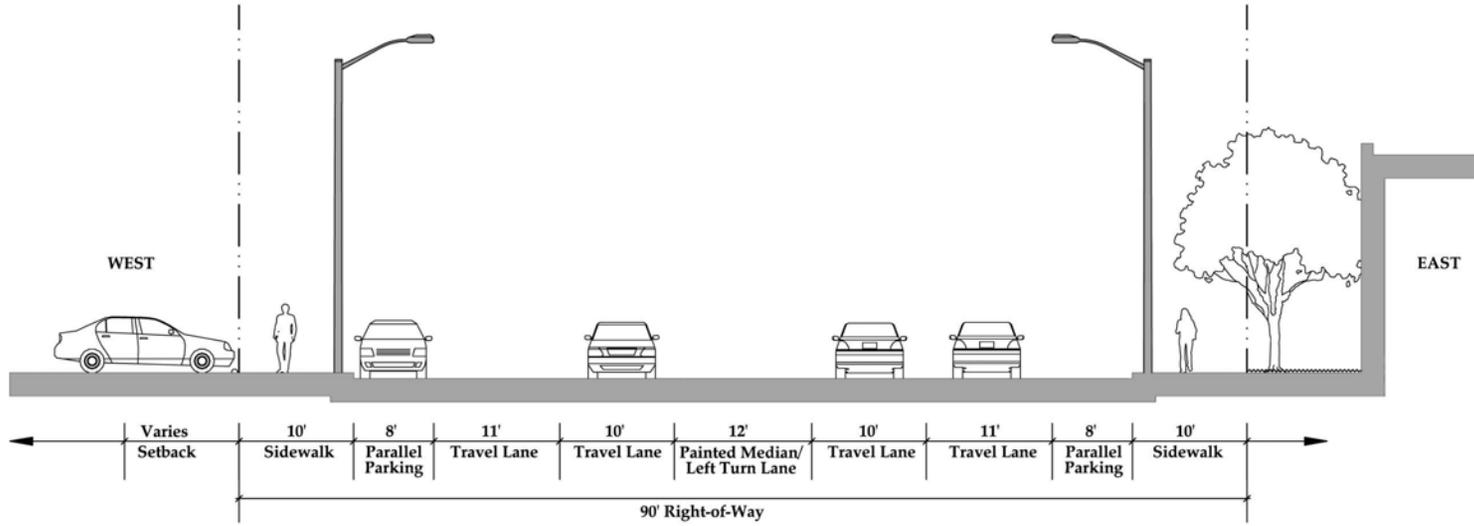
Notable area features include the west gateway to Downtown at East Campbell Avenue and a number of large sites with unique developments. These include the Campbell Community Center, St. Lucy's Church and School, Campbell Plaza shopping center, The Winchester light rail station, the Heritage Village Apartments and Offices, and the Campbell United Methodist Church. The Campbell Community Center is the focal point of the Winchester Boulevard planning area. This former high school, built in 1938 and designed by W. H. Weeks, is one of the city's historical and architectural jewels, and is home to the Heritage Theatre.

The Heritage Village offices and Heritage Village apartments are located just north of East Campbell Avenue, directly across Winchester Boulevard from the Campbell Community Center. These three developments all feature a Mediterranean building style, with white stucco walls, red tile roofs, casement windows and related elements that combine to create an area that exhibits the corridor's most distinctive architectural character.

St. Lucy's Church and School facility and Campbell Plaza anchor the southern end of the master plan area, and along with the Summerwinds Nursery, encircle the Winchester light rail station. The station is on the Mountain View-Winchester line, which provides service to Downtown San Jose and Mountain View, with connections to the San Jose International Airport and other light rail and bus lines.



Winchester Boulevard (North of Campbell Ave) Typical Existing Conditions



Winchester Boulevard (South of Campbell Ave) Typical Existing Conditions



Front Setback Conditions

III. Master Plan Vision

Vision Concept

The Winchester Boulevard corridor is gradually evolving from a “commercial strip,” with one-story commercial buildings and surface parking lots, to a higher-value boulevard street with higher density office, infill residential development, and locally oriented commercial businesses. The corridor’s focus on drive-by and convenience commercial development is shifting to a more destination-oriented development pattern. Some of this relates to the expansion of Silicon Valley-related enterprises, such as Netflix in Los Gatos, as well as community development efforts such as the revitalization of Downtown Campbell. Winchester Boulevard provides access to Downtown Campbell’s “west gateway” at East Campbell Avenue, and some of the interest in the Winchester corridor is likely related to a spillover of interest in Downtown.

The overall goal of the Master Plan is to establish a vision that guides new development and major renovations to create an attractive boulevard street that adds value to the community as a whole. A key aspect is relating policies for building intensity, height, and site improvements to local site conditions and the adjacent neighborhood context. Ideally, new private development and city-sponsored capital improvements will combine to create a more pedestrian-friendly environment as well, with mixed-use buildings lining the street edge and an improved streetscape that defines Winchester Boulevard as an attractive and memorable public place.

Wider sidewalks will be buffered from passing traffic with consistent street trees, and other amenities, and improved curbside parking and pedestrian-oriented street lighting in some locations. Public street improvements should support multi-modal

transportation opportunities for vehicles, bikes and pedestrians, as well as provide aesthetic improvements in the form of lighting, landscaping, Downtown gateway signage, and street furnishings. Existing overhead utilities should be relocated below grade.

Development standards, design guidelines, and public improvements recommendations reflect the three geographic subareas described in Chapter I: “Area 1” extends from Camden Avenue on the south to Budd Avenue; “Area 2” extends from Budd Avenue to Campbell Avenue; “Area 3” extends from Campbell Avenue north to Rosemary Lane, the City’s northerly City limit.

Two types of policies are established. “Mixed-Use/Civic Boulevard” policies apply to Areas 1 and 3, where parcel size and street frontage conditions are generally consistent. These policies promote a mixed-use development pattern that includes a combination of “freestanding” office, residential and commercial development. Side setbacks, a frontage landscape strip (allowed by the lack of curbside parking), and, over time, installation of a central landscaped median island will combine to create an open, “green” character for these areas.

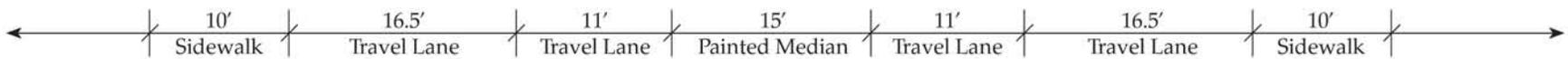
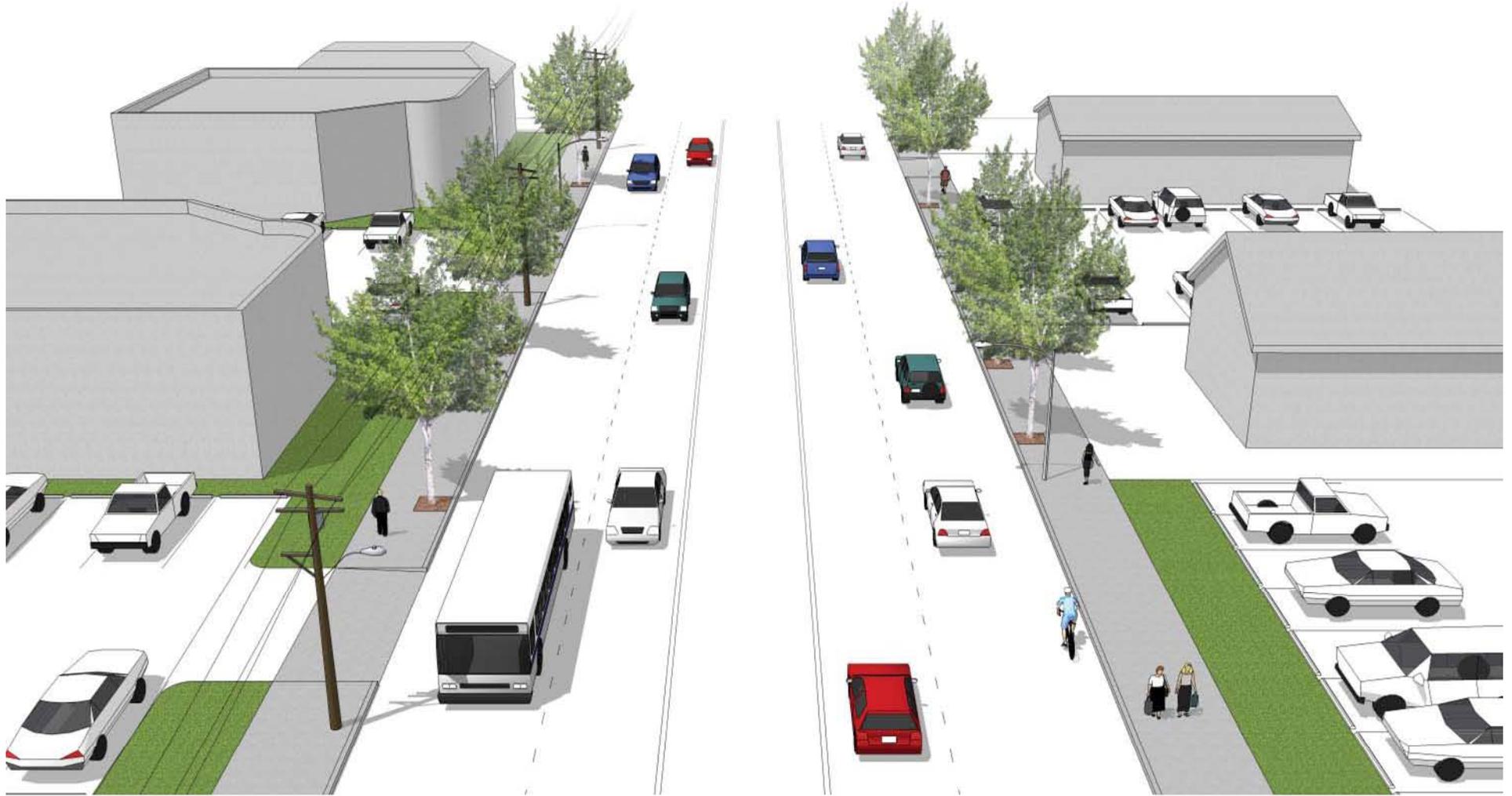
“Neighborhood Commercial Boulevard” policies apply to Area 2, and promote a land use mix that is more oriented to and more sensitive to adjacent neighborhood areas. First floor commercial space is required. Proposed public improvements focus on storefront district-type street amenities.

Traffic and parking reviews that would consider neighborhood impacts are suggested for new development projects in both areas.

Building heights are recommended to vary up to a maximum of 4 stories and 55 feet in Areas 1 and 3, and up to three stories and 45 feet in Area 2. In Area 2 this could accommodate a village scale of development with up to two floors of residential and/or office space above the ground floor commercial frontage. In both policy areas,

surface parking is recommended to be located behind, adjacent to, or below buildings, not in front.

The vision sketches on the following pages illustrate typical existing conditions and proposed policy recommendations for the two areas.



Current Conditions (Area 1 and Area 3)

- Parking lots along street frontage
- Traffic speed deters pedestrians
- Inconsistent street trees
- Overhead power lines
- Painted median
- City standard street lighting (few street lights on utility poles)
- No on-street parking (except for a few locations on the west side of Area 3 and on the east side of Area 1)
- Bicycles: bike route

Winchester Boulevard – Area 1 and Area 3 Existing



Proposed Conditions (Area 1 and Area 3)

- Buildings along street frontage; parking behind
- Planting strip acts as buffer between pedestrians and traffic
- Consistent street trees
- Underground power lines
- Planted median
- City standard street lighting
- No on-street parking except on east side of Area 1
- Bicycles: bike route

Winchester Boulevard – Area 1 and Area 3 Proposed



Current Conditions (Area 2)

- Parking lots along street frontage
- Traffic speed deters curbside parking and pedestrians
- Inconsistent street trees
- Overhead power lines
- Minimal crosswalks
- City standard street lighting
- On-street parking
- Bicycles: bike route

Winchester Boulevard – Area 2 Existing



Proposed Conditions (Area 2)

- Buildings along street frontage; parking behind
- Bulb-outs/parking pockets slow traffic
- Consistent street trees
- Underground power lines
- Evaluate additional crosswalks at signals
- Combined pedestrian-oriented/street lighting standards
- On-street parking
- Bicycles: bike route

Winchester Boulevard – Area 2 Proposed

Frontage Public Streetscape Improvements

Curbside parking should be retained in Area 2 to support storefront commercial businesses. Widened frontage sidewalks should be created by building setbacks as part of new development, consistent with the vision for a pedestrian-oriented commercial area. A width of 15 feet is recommended (10' in public right of way and 5' on private property), with wider areas at bulb-outs. Sidewalks are narrower than this today, and creating wider sidewalks throughout the Master Plan Area is likely to be a gradual effort, combining incremental capital improvements with larger scale development projects.

Except at intersections, existing “cobra-head” highway lights in Area 2 should be replaced with lower, more closely spaced combination pedestrian-oriented/street light standards, unique to this area. Streetlights are recommended to be approximately 14 to 16 feet in height and ornamental in character. The City should verify that future light placement, pole height, bulb type and lighting levels are appropriate for street conditions.

In Areas 1 and 3, a 17 foot wide frontage/setback area is recommended, similar to that recommended for Area 2. However, the additional space created would instead be used to create a landscaped planting strip 10 feet wide and a sidewalk area a minimum of 7 feet wide, within the public right of way. Deciduous shade trees are recommended. City standard street lighting could remain for these areas, with the focus of improvements on a landscaped median.

Pedestrian Crossings

Corner sidewalk bulb-outs are recommended to be evaluated at all intersections to reduce street crossing distances and improve pedestrian visibility. New bulb-outs would include expanded, ADA-compliant sidewalk ramps. Corner curb bulb-outs should generally

have a minimum radius of 20' to facilitate truck and emergency vehicle turning movements. Bulb outs should include a decorative paving material or an alternate color or paving pattern. Bulb out locations are to be reviewed with consideration given to impacts on adjacent neighborhoods and U-turn access. In some instances, for example the southwest corner of Winchester Boulevard and Cherry Lane, standard curb returns may be necessary to accommodate existing traffic patterns such as traffic from nearby Campbell Middle School.

Highly visible crosswalks are recommended to enhance the street crossing experience, alert motorists, and particularly in Area 2, project a slow-traffic character for the street. Bulb-out and enhanced crosswalk locations are depicted on the “Streetscape Improvement Concept” diagram in Chapter V. A “continental” or “zebra” paint pattern is recommended for crossings on Winchester Boulevard; however, the City may consider other treatments as more detailed improvement plans are prepared. “Countdown” pedestrian signals are recommended for installation at all signalized intersections.

Community Center / Downtown Link

These two important community destinations are currently cut off from one another by Winchester Boulevard. There are two aspects to improving the pedestrian connection. One is improvement to the Winchester Boulevard crossing, with improved crosswalk markings and corner bulb-outs. The other is creating a more direct route between Downtown and Winchester along Civic Center Drive and along the frontage of Heritage Village. New sidewalks, crossings, and possibly improved lighting are recommended for this area, as well as the frontage on the south side of Campbell Avenue between Winchester Boulevard and Jeffers Way.

IV. Development Standards & Design Guidelines

The *Campbell General Plan*, updated in 2001, provides clear direction regarding land use within the *Winchester Boulevard Master Plan Area*. The area is planned for mixed-use development, with ground floor commercial and office or residential above at a density of up to 27 units per acre. Development Standards and design guidelines address massing, architectural styles, height, and the general development pattern that would best accomplish the physical changes needed to create an attractive transition between the historic Downtown core, Community Center, and areas north and south along Winchester Boulevard.

The Winchester Master Plan includes Policy Areas 1, 2, and 3, as indicated by the “Land Use and Development Guidelines” plan on the next page. Generally, Areas 1 and 3 (north of Campbell Avenue and south of Budd Avenue) have larger lots, allowing for bigger redevelopment projects, and the public improvement standards call for a boulevard treatment similar to other major arterials in the City, such as Bascom Avenue. Area 2 has smaller lots, similar to the Downtown, so both the private and public development standards and design guidelines mirror those established for the Downtown.

The Master Plan area should be compatible in character with Downtown, but not attempt to duplicate it in form. Unlike Downtown, for example, the street width is 68-70 feet from curb to curb; in Downtown it is 40 feet. This allows the Master Plan area to accommodate larger buildings more gracefully, without creating a tunnel effect along the street. Parcels adjacent to low-density residential areas will have increased setbacks to minimize the impact of new development.

While a somewhat increased scale will differentiate Winchester Boulevard from the Downtown Core, architecturally the two areas will be compatible. Many of the standards guiding development in

the Downtown Core are incorporated herein, including those pertaining to land-use. Development along Winchester Boulevard is intended to expand on and complement Downtown’s small-town feel.

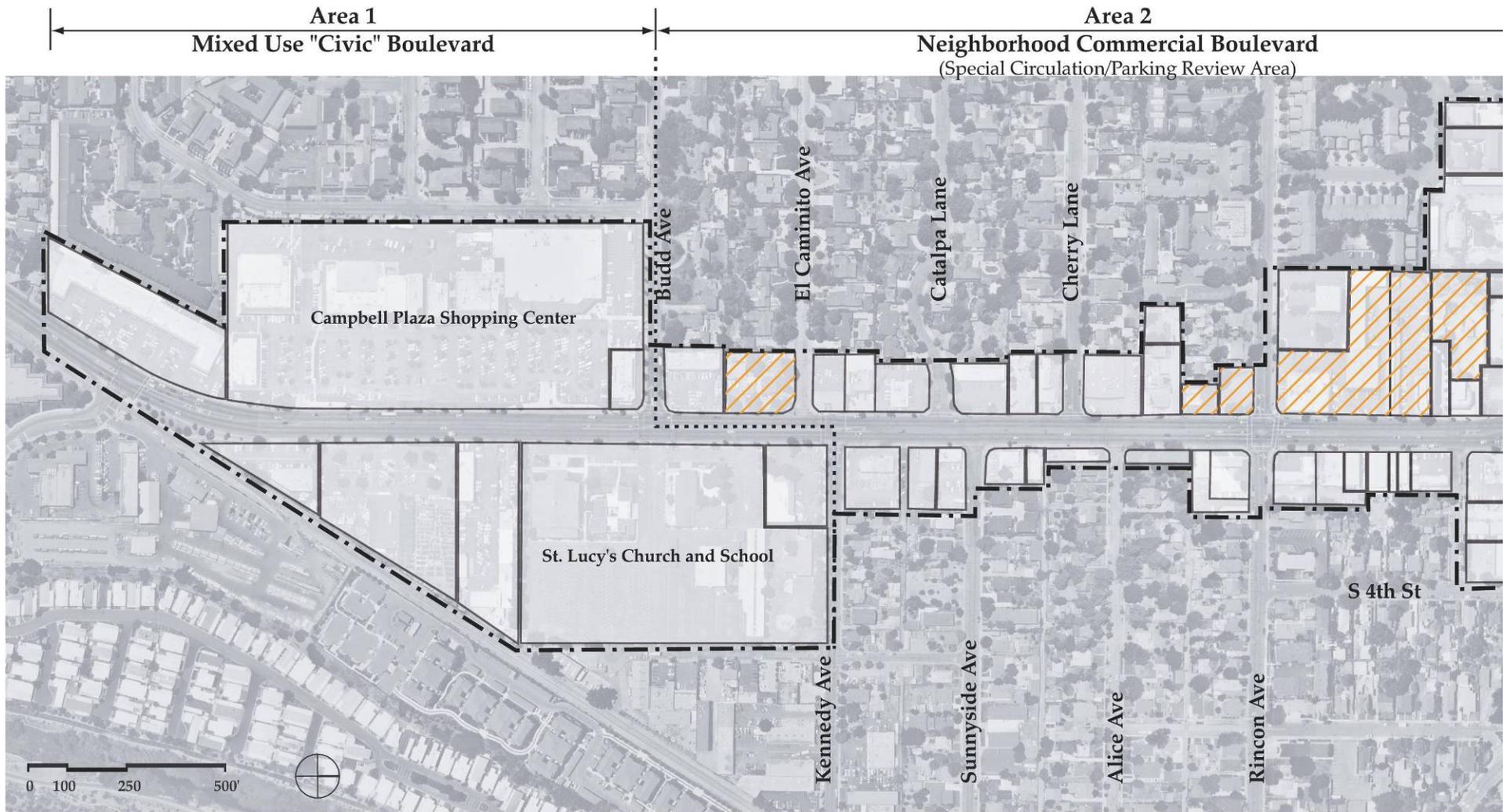
Building heights in the corridor will reflect a 2:1 building height/lot depth ratio that, a) requires lower heights adjacent to single family residential areas, b) encourages parking behind frontage buildings, and c) results in variety in building heights.

The Planned Development process will help ensure that new development is high quality. This means a variety of building forms, quality materials, a pleasant streetscape, and variety of businesses. Many properties along the Winchester corridor are ripe for redevelopment. An Area Plan will help ensure a more orderly transition from the current strip-mall style of development to the pedestrian-oriented and lively environment envisioned by the General Plan.

DEVELOPMENT STANDARDS

Permitted, Conditional and Prohibited Uses

The subject parcels in this Plan Area are predominantly zoned “Planned Development.” The Planned Development Zone District regulations do not specifically list Permitted, Conditional and Prohibited Uses. Instead, allowed uses in the Planned Development district are determined by referencing the zone district which corresponds with the General Plan designation. The subject parcels predominantly have a General Plan designation of Central Commercial (C-3), therefore Permitted, Conditional and Prohibited Uses shall be those set forth in the C-3 zoning district.



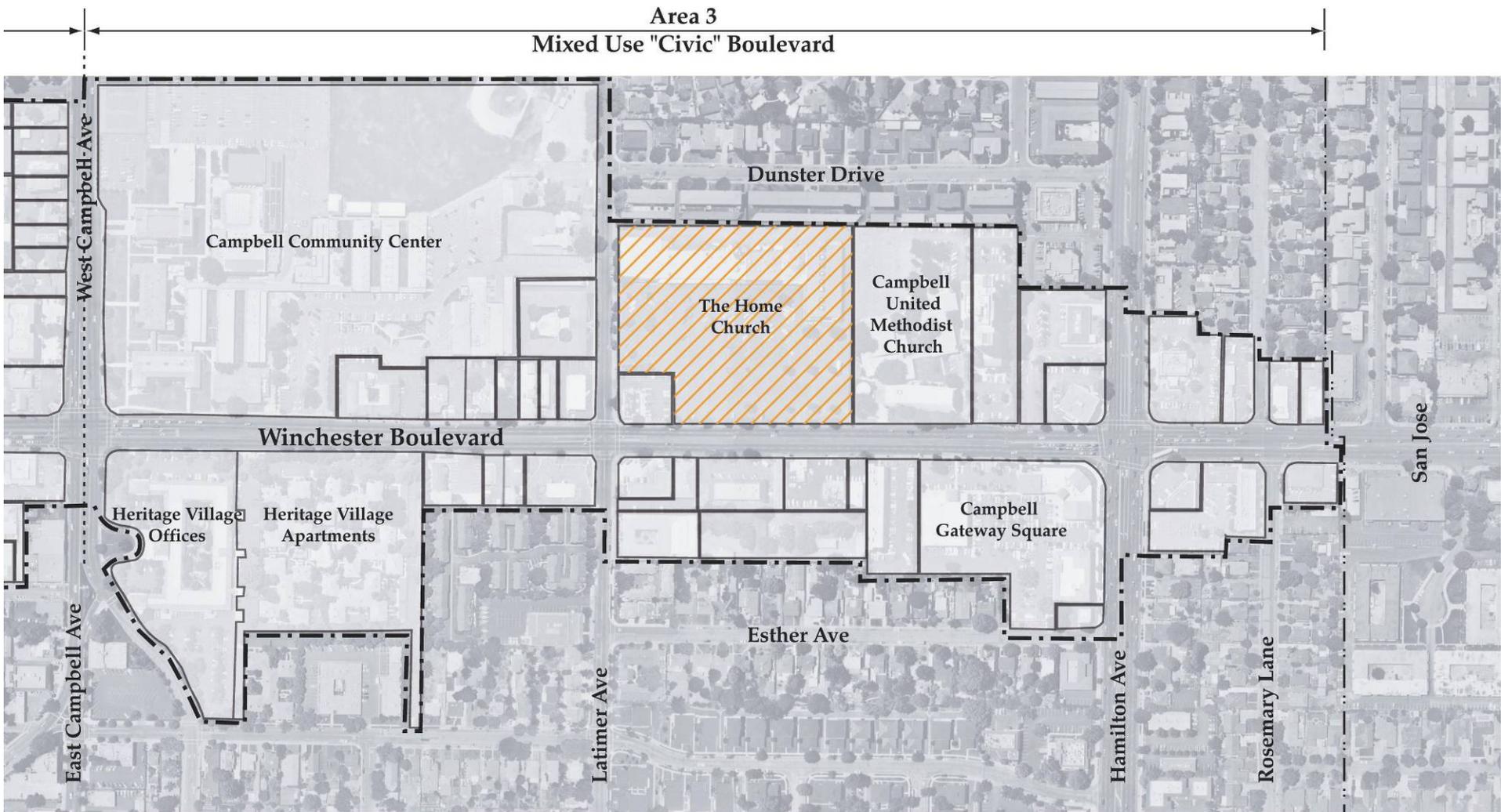
Neighborhood Commercial Boulevard (Area 2)

- **First Floor - Commercial/Retail w/ Neighborhood Focus**
Strongly Recommended
- **Upper Floors - Residential, Professional Office Recommended**
- **Building Setbacks**
Front: 5' from ROW min.
Rear: 8' Rear to Parking or Building (see Building Height)
Side: 5' along Street Frontage
- **Building Height**
45' / 3 Story Max.
Setback / Height from Rear Property Line 2:1

- **Building Frontage**
120' Max. Building Frontage (ie unbroken)
Main Entrance Facing Winchester Blvd/
Campbell Ave Required
- **Neighborhood Traffic/Circulation**
Side Street Traffic and Parking Examined on
Case by Case Basis
- **Bicycles**
Share Curb Lane with Vehicles

- **Parking**
On-Street Parking Allowed
Location - Rear of Building
Quantity - Per City Code
Landscape - Shade Trees Required, Min. 1/4 Stalls
- **Frontage and Streetscape**
Widened Sidewalk to 15'
Street Trees at 40' On Center with Tree Grates
Bulb-Outs and Other Improvements to be Considered
Combined Pedestrian/Street Light Standards
Underground Utilities

Land Use and Development Guidelines



Mixed-Use "Civic" Boulevard (Area 1 and Area 3)

- First Floor - Commercial Frontage, Residential, or Office
- Upper Floors - Commercial, Residential, or Office
- Building Setbacks
 - Front: 15' from ROW recommended (7' min)
 - Rear: 8' Rear to Parking or Building (see Building Height)
 - Side: 10', w. 20' Between Buildings

- Building Height
 - 55' / 4 Story Max.
 - Setback / Height from Rear Property Line 2:1
- Building Frontage
 - 200' Max. Building Frontage (ie unbroken)
 - Main Entrance Side or Facing Winchester Blvd Required
- Bicycles
 - Striped Edge Line / Bike Route

- Parking
 - No On-Street Parking (Except East Side of Area 1)
 - Location - Side or Rear of Building Primary; Minimal in Front
 - Quantity - Per City Code
 - Landscape - Shade Trees Required, Min. 1/4 Stalls
- Frontage and Streetscape
 - Planting Strip 10' / Sidewalk 7'
 - Street Trees at 40' On Center
 - Bulb-Outs and Other Improvements to be Considered
 - City Standard Street Lights
 - Underground Utilities

Proposed Development Project

Land Use and Development Guidelines

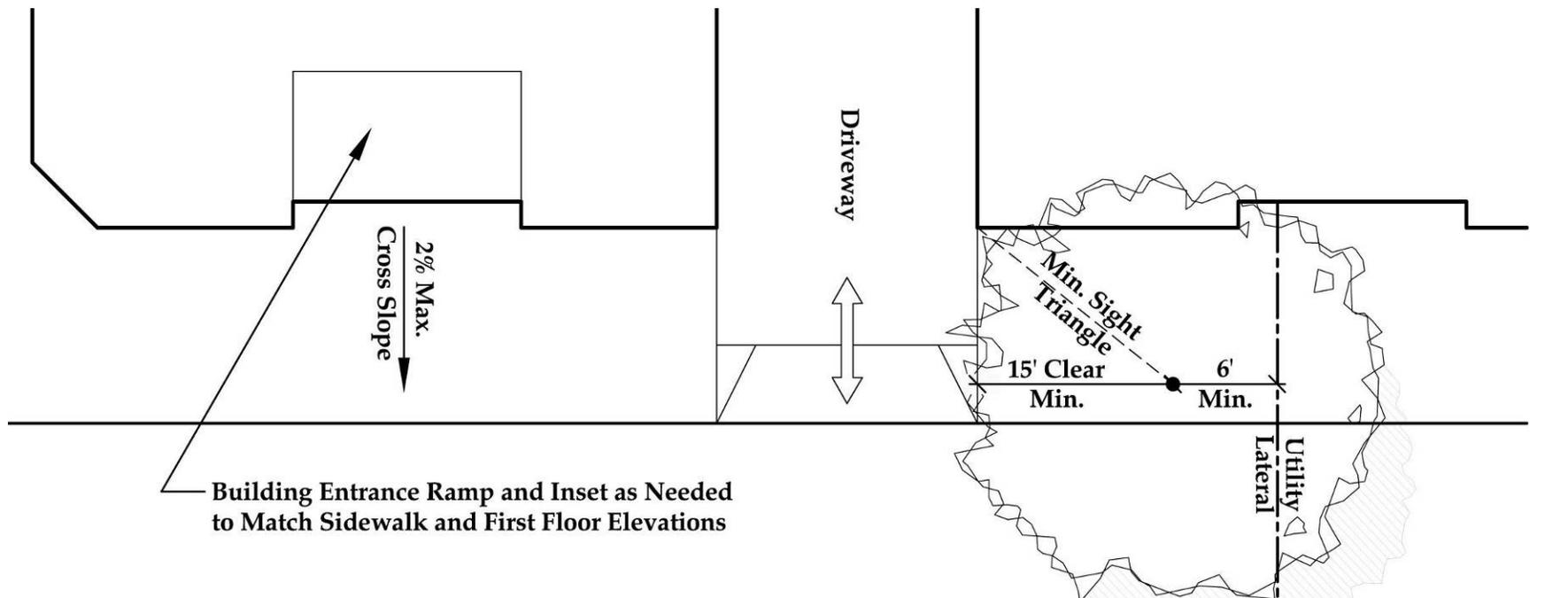
The Plan Area also includes parcels designated for General Commercial, High Density Residential and Institutional at the northern boundary of Area 3; Public Facilities, High Density Residential and Office at the southern boundary of Area 3; and Public Facilities in Area 1.

In general, the vision for this Plan area shall be ground floor retail/restaurant, with upper floor residential/office. It is expected that a variety of ground floor retail businesses and eating establishments shall be maintained to achieve a balanced and distinctive pedestrian-oriented experience, without an over-concentration of any one type of use. Residential uses at ground level may be considered when the property is deep enough to locate residential uses behind commercial buildings that face Winchester Boulevard.

Sidewalks

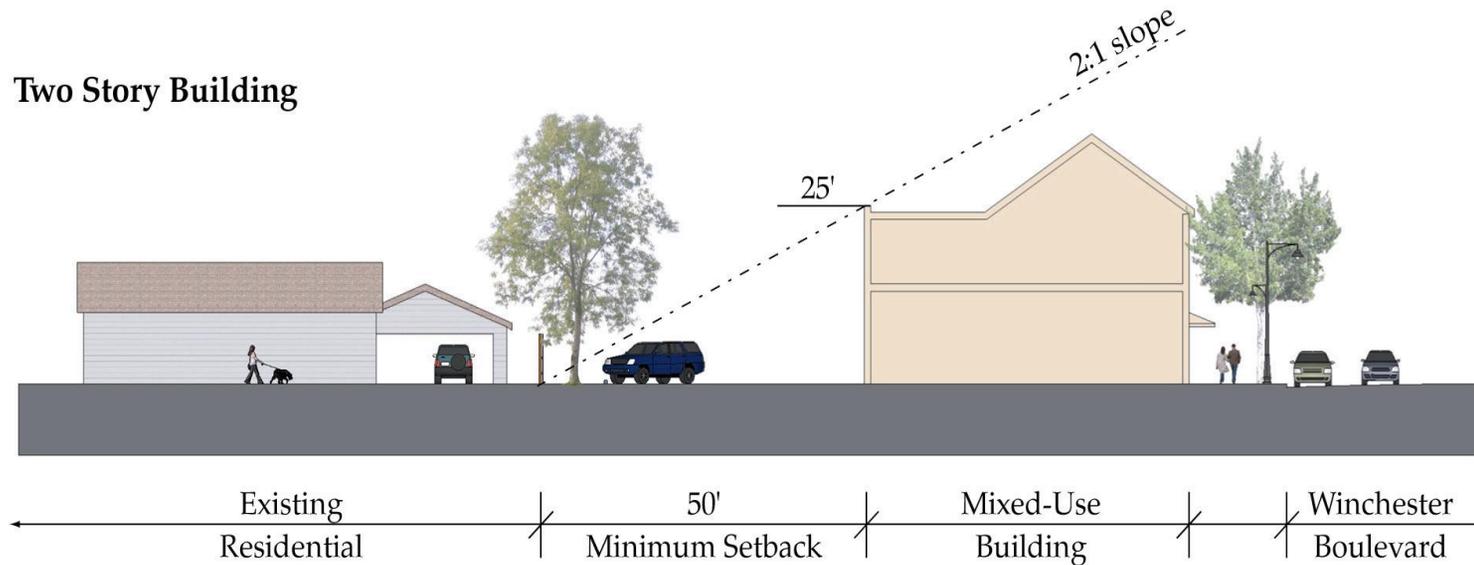
In conformance with the *Winchester Boulevard Master Plan*:

- Include sidewalks of sufficient depth to accommodate outdoor seating for restaurants or cafés in Area 2. This may include the use of private property. Outdoor seating shall be in compliance with CMC Section 21.36.150, Outdoor Seating.
- Include street trees in conformance with this Master Plan and applicable Zoning and General Plan provisions.
- Develop sidewalks with corner “bulb-outs”, where feasible.
- Maintain a maximum of 2% sidewalk cross-slope at all building entries.



Sidewalk Standards Diagram

Two Story Building



Building Height

- Maximum building height shall be 55 feet in Areas 1 and 3, and 45 feet in Area 2. Buildings in the Master Plan area shall vary in height to achieve an eclectic rhythm, both within and between buildings. This can be achieved by “wedding-caking” – i.e., locating taller building elements along the street frontage -- and other design approaches. Wedding-caking shall not preclude taller architectural elements at street corners. New construction shall require a Pre-Application Review by the Planning Commission, including a massing study.
- Minimum building height shall be two stories in Areas 1 and 3, and one story in Area 2.
- Ground floor interior finished ceiling heights shall be a minimum of 15 feet to accommodate retail operations.
- A 2:1 ratio of lot depth to building height is recommended as depicted on the “Building Height” and “Setback Envelope” diagrams in the following pages.

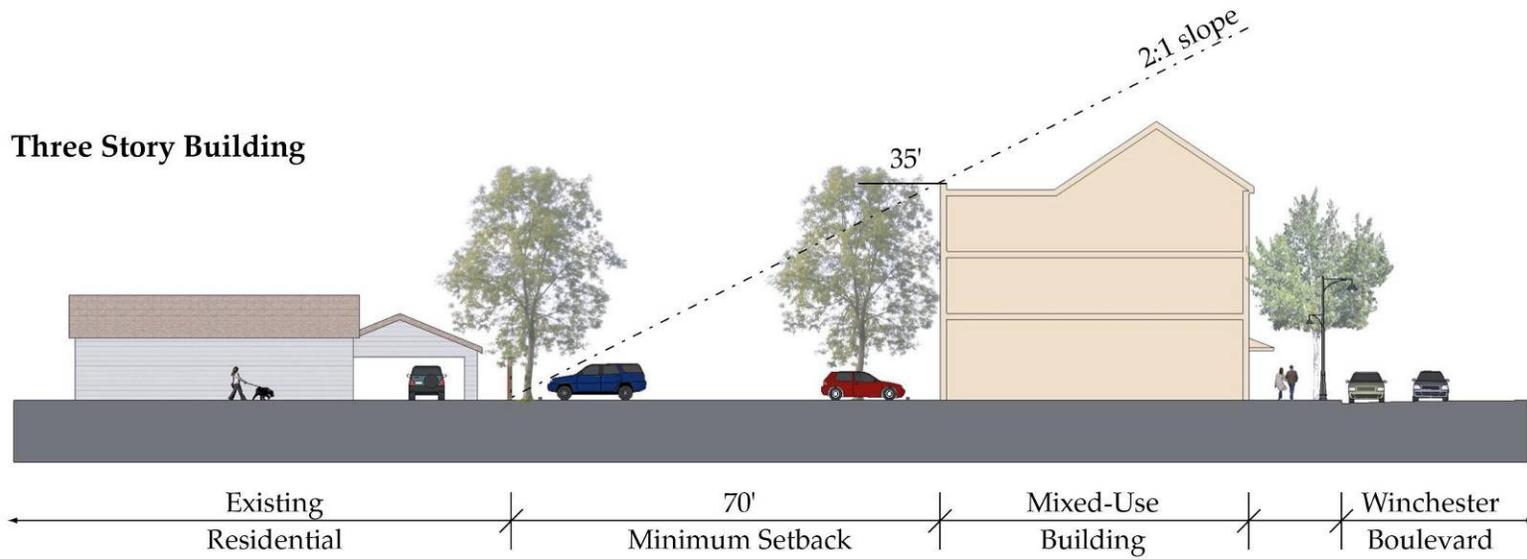
Floor Area Ratio (F.A.R.)

- Maximum FAR shall be 1.5 (exclusive of residential).

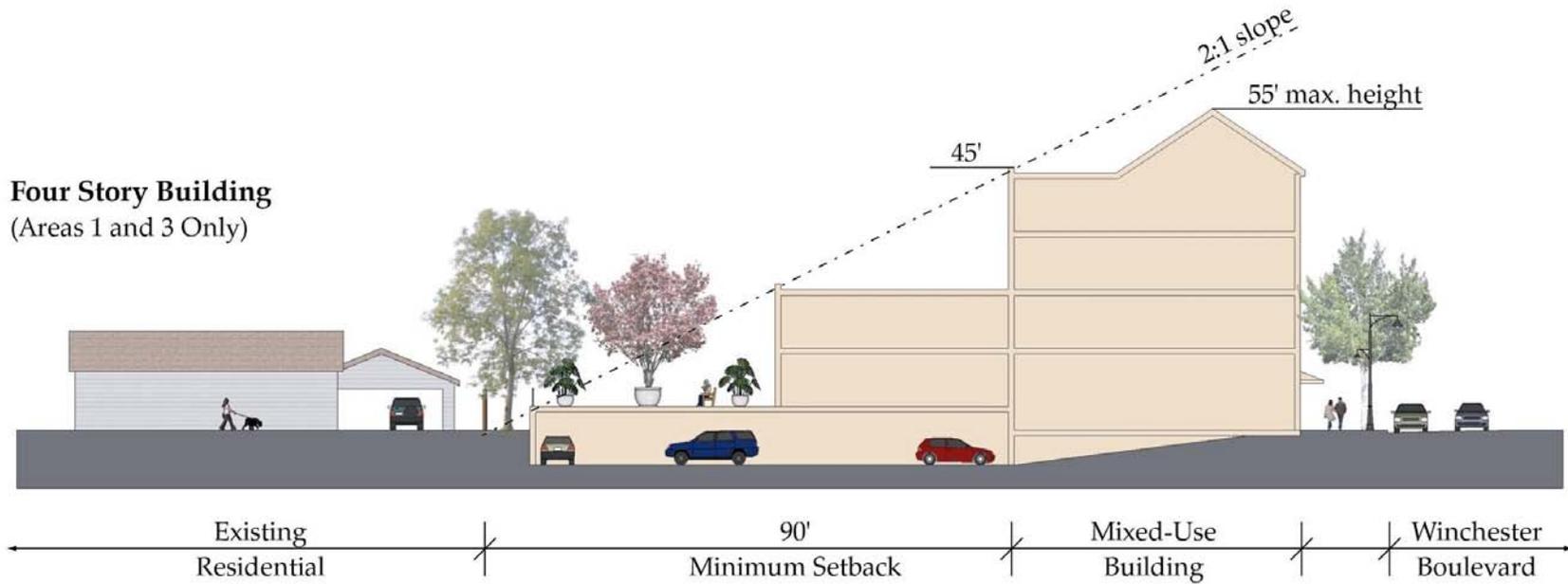
Front Setbacks – as measured from the rear of abutting sidewalks.

- The first floor shall front the property line utilizing a five (5) foot setback to create a pedestrian-accessible retail experience in Area 2. A similar seven (7) foot minimum shall apply to Areas 1 and 3. Deeper setbacks along the streetscape shall be considered given site and development conditions.
- Adjustments to setback requirements related to the use of special architectural features or other elements may be approved by the decision-making body

Three Story Building



**Four Story Building
(Areas 1 and 3 Only)**



Building Height / Lot Depth Diagram

Rear Setbacks – as measured from abutting property line.

- See Setback Envelope Diagram – page 27.

Side Setbacks

- Minimum shall be zero (0) feet in Area 2, except as necessary for pedestrian or vehicular access ways. Minimum shall be ten (10) feet in Areas 1 and 3.
- Deeper side yard setbacks shall be provided at corner side yards and where side yards exist adjacent to properties zoned for single family residential.

Site Access

- Pedestrian Access – Every building and tenant space shall provide a main entrance directly adjacent to the sidewalk frontage in Area 2. Entrances may need to be recessed in order to provide a consistent 2% maximum sidewalk cross-slope and flush entryway.
- Driveways/Curb Cuts – Minimize the number of driveways/curb cuts by development of shared parking lots where possible. Limit the number of driveways/curb cuts to a maximum of 1 two-way curb cut, or 2 one-way curb cuts per building in Area 2.

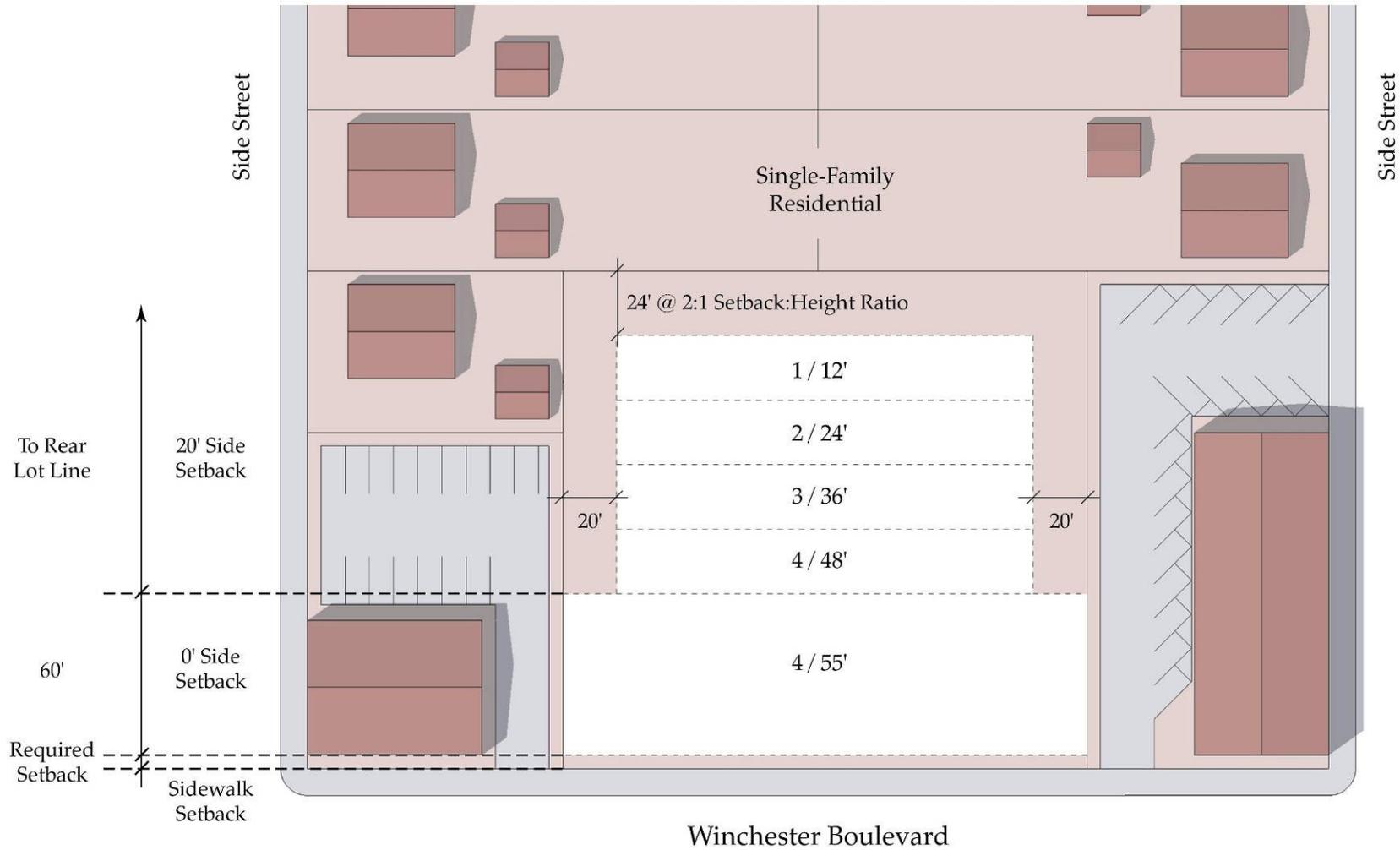
Service Areas and Access Drives

- Centralize and share service areas/access drives wherever possible.
- Service areas (including dumpsters and similar facilities) shall be accessed from side or rear parking areas, and shall be screened by architectural walls, fencing, and/or planting, as appropriate.

- Consider residential adjacencies when designing service areas/access drives.

Parking

- Minimum Requirements: All new developments are subject to existing city parking requirements. Adjustments to parking requirements may be approved by the decision making body pursuant to CMC 21.28.050.
- No new surface parking shall be developed fronting Winchester Boulevard in Area 2.
- Surface parking shall be permissible only in the rear of new developments in Area 2.
- The rear of surface lots in Area 2 shall be screened with an attractive fence or wall that complements the material of the principal building. Walls or fences shall not exceed a height of 6 feet.
- All surface lots shall utilize trees or other landscaping to provide shade throughout the lot.
- Where rear parking lots are provided, access ways shall be well lit and landscaped.
- As most new development along Winchester Boulevard will be mixed-use, and some of it will be within ¼ mile of light-rail station—or both—there is a high likelihood that many projects will request a reduction in parking. The Planning Commission may consider reductions in parking on a case-by-case basis when it finds that: reduced parking is warranted either due to the mixed use nature of the development (which could accommodate shared parking), the development is within ¼ mile of light-rail station or for another supportable reason.



Setback Envelope Diagram

Structured Parking

- Below grade parking is encouraged where feasible.
- Parking structures located on Winchester Boulevard should incorporate retail storefronts at the ground level along front-facing elevations to prevent the creation of “dead zones” along the street. Fronting office or residential space may be appropriate in Area 3.
- Prohibit podium parking designs that would raise the ground floor along the Winchester Boulevard frontage; frontage commercial space should be entered at-grade.

Utilities

- Utility boxes and equipment shall be undergrounded to the extent feasible and practical.

DESIGN GUIDELINES

Ground Level Treatment

The ground level of buildings on Winchester Boulevard shall include design features, such as retail display windows and building articulation, which are attractive and pedestrian-oriented. Particular attention should be given to craftsmanship and detailing within the pedestrian's range of experience. This is particularly important in Area 2. The use of special storefront detailing, special materials, planters, outdoor seating, decorative pavers, flags and banners should be included to reinforce the pedestrian nature of the street.

Buildings facing Winchester Boulevard should be designed to maintain a development pattern that promotes activity and an active pedestrian-oriented environment. Recessed buildings, particularly on corner lots, may be allowed to provide for outdoor dining, public areas, or design excellence as determined to be of benefit to the overall Winchester Boulevard corridor.

Façade Treatment and Massing

Consistent with the present scale and character of Downtown, large, uninterrupted expanses of horizontal and vertical wall surface should be avoided. Large buildings should be divided into multiple storefronts or similarly scaled elements to complement smaller property divisions. Building facades should reflect relatively narrow increments of development (25' to 50') with variation in building planes. Eclectic rhythm shall be accomplished by varying the design of building fenestration and materials between buildings.

Corner parcels are encouraged to incorporate special features such as rounded or cut corners, special corner entrances, display windows, corner roof features, etc. Additionally, the massing of development should be designed to reflect the Downtown's diverse character and scale through variation in roofline, building plane and

materials. Building elements that add scale and interest such as second-story bay windows, parapets, and cornices, are encouraged. Special attention to detail should be given to elevations that include a mix of finishing materials, façade ornamentation, lighting, flower boxes and/or storefront articulation.

Ground floor windows should be attractive, inviting, and enticing to passersby. Windows should be inset generously from the building wall to create shade and shadow detail. Retail establishments should utilize expansive storefront windows to provide the appropriate setting for displaying and marketing retail merchandise.

Building Materials

Building materials should be of a high quality. A mix of materials is encouraged to continue the eclectic rhythm of Downtown.

Signs

All signs should be of a high quality and complement current signage in Downtown, with respect to size, color, and design. All signs in Area 2 should substantially conform to guidelines specified in *Sign Standards for Historic Downtown Campbell*.

V. Recommended Public Improvements

Recommended public improvements are described below and indicated by the “Streetscape Improvement Concept” diagram and the enlarged plans on the following pages, as well as the “vision sketches” contained in Chapter III.

A key element of the Master Plan is improving the environment along Winchester Boulevard for local businesses, patrons, employees and residents, while improving the overall image and function of the street for the community-at-large. There are two dimensions -- creating an attractive environment for walking and driving along the street, and improving safety and convenience for pedestrians at street crossings.

The Streetscape Improvement Concept incorporates both types of improvements, as described below. Street and frontage improvements are described first, followed by intersection and pedestrian crossing improvements and recommendations for the Winchester/Campbell intersection.

Frontage Improvements

The overall vision for Winchester Boulevard includes landscaped median islands in Areas 1 and 3, and enhanced and widened sidewalks in Area 2. These and other improvements recommended by the Master Plan have different means and time frames for implementation. For example, widened sidewalks, curbside planting strips, new street trees, etc., may be installed incrementally as new private development occurs. Installation of medians, on the other hand, requires a coordinated design and construction approach led by the City, as medians can not generally be installed incrementally.

Existing curb lines should be retained for the most part. This reduces the significant costs associated with reconstruction of storm drainage facilities. However, corner curb bulb-outs and other features are proposed to alter curb lines at a number of intersections. Frontage improvements are described in more detail below.

Utility Undergrounding – An Underground Utility District should be considered along the frontage of Winchester Boulevard, from Hamilton Avenue on the north to Budd Avenue on the south. The City is pursuing the funding needed to underground power lines in this area within the next few years. This project will greatly improve the visual quality of the street, both by eliminating unsightly power lines and by providing more room for the planting of significant shade trees.

Utility undergrounding will require installation of utility vaults along the street frontage, as illustrated in the “Typical Street Frontage Improvements” diagram. Location of large underground utility vaults should be coordinated with the Master Plan. Preferably, vaults would be located outside the pedestrian travel way.

Pedestrian-Oriented Street Lighting – Combined pedestrian-oriented/street lighting is recommended for Area 2, and ideally this lighting would be installed in conjunction with the utility undergrounding for the area, rather than in an incremental fashion with proposed developments.

It is recommended that light fixtures be ornamental in nature to complement the storefront commercial and infill residential development anticipated for the area. However, the lights in this area should be a different model than those installed in Downtown; a “high/low” fixture is illustrated in the vision sketch for Area 2, but the actual type of fixture remains to be selected by the City based on lighting conditions, energy efficiency, design character and other factors.



- Existing Bus Stop
- Existing Signalized Intersection
- Existing Crosswalk
- Proposed Street Trees

0 100' 250' 500'



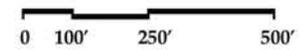
Streetscape Improvement Concept

No On-Street Parking
Area 3

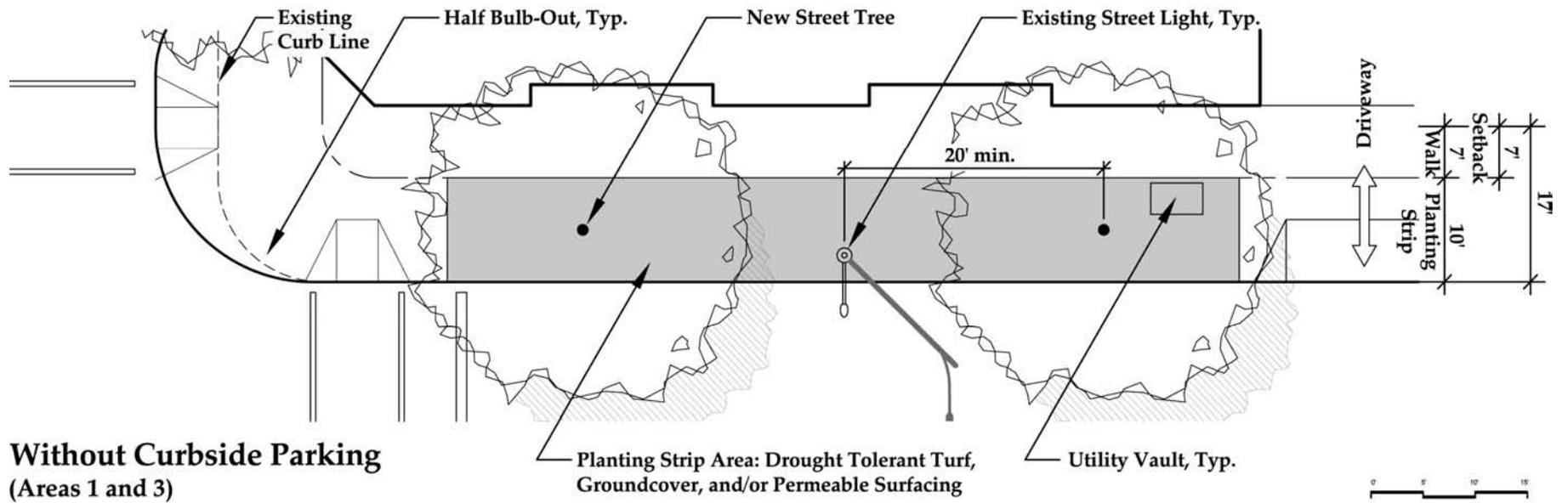
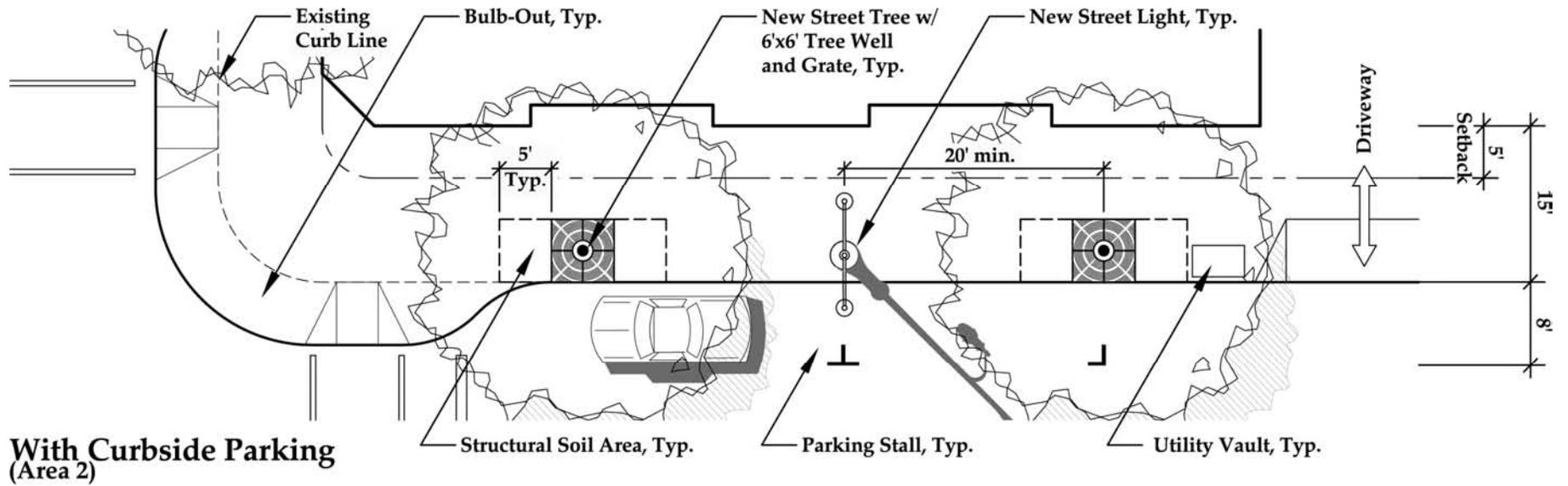


 Potential Full Bulb-Out

 Potential Partial Bulb-Out



Streetscape Improvement Concept



Typical Street Improvements

Street Trees – Deciduous shade trees are recommended to line street frontages throughout the Master Plan area. Species should be high-branching and broad-spreading to provide shade and define the street as a public space, and deep-rooted to minimize damage to sidewalk and street paving surfaces. Street trees should be planted at approximately 40 feet on center, in large tree wells that are a minimum dimension of 4 feet by 4 feet. At time of planting, trees should be a minimum 24" box size, with 36" box recommended. Tree grates should also be installed in Area 2.

As depicted by the "Typical Street Frontage Improvements" diagram, street trees and streetlights should be located at least 20 feet apart. Structural soil trenches are recommended as part of street tree installation to expand rooting area while preserving sidewalks and adjacent paving surfaces. Trenches should be approximately 36 inches deep and the same width as tree wells.

Corner Curb Bulb-Outs – Bulb-outs should be considered at all intersections, as possible, given vehicle turning conditions and potential impacts to adjacent neighborhoods. Developer contributions toward installation of bulb-outs should be considered for all new development and major renovations.

Bulb-outs would actually serve two functions on Winchester Boulevard: 1) the typical function of enhancing the safety and visibility of pedestrian crossings, and 2) a traffic calming effect. Bulb-outs also provide space for future sidewalk amenities and new ADA-compliant curb ramps.

As depicted by the "Streetscape Improvement Concept," bulb-outs generally are recommended at existing signalized crosswalk locations, with half bulb-outs recommended for frontage locations that do not have curbside parking and/or where right turn lanes are required.

Curbside Planting Strip - A curbside planting strip 10 feet wide is recommended for Area 1 and Area 3. This planting strip is a key element of the "Mixed-Use Boulevard" policy concept, combining with future median islands to create a greener and more gracious character for these areas. Planting strips should be installed in combination with a minimum 5 foot building setback for all new development, as depicted by the "Typical Street Frontage Improvements" diagram. As noted above, deciduous shade trees planted at approximately 40 feet on center are recommended. Drought tolerant turf, groundcover, and/or permeable surfacing are recommended to maintain visibility for motorists and pedestrians. The City should define a limited palette of these materials, as well as preferred street tree species.

Street Improvements

New Pedestrian Crossings – New pedestrian crossings should be considered at existing signalized intersections where only one leg of the intersection currently has a crosswalk across Winchester Boulevard. This condition occurs in Area 2; examples include Budd Avenue and Kennedy Avenue. Potential crosswalks at these locations should be reviewed to determine and evaluate any impacts created by the modification of the existing signal phasing plan. Striped shoulder areas could allow for crosswalk-related bulb-outs in Areas 1 and 3 as well. Possible locations include the entrance to the light rail station in Area 1, and the Latimer Avenue intersection in Area 3. Special paving or "continental" painting could be used to highlight all crosswalks on Winchester Boulevard.

Landscaped Median Island – Winchester Boulevard contains a continuous 12- to-15-foot left turn lane/painted median that could accommodate a landscaped median island of comparable width. Policy Areas 1 and 3 have a minimal number of side streets and corresponding left turn requirements, and landscaped islands are therefore recommended for these areas. (Area 2 contains offset side

streets and short blocks; installation of medians would severely disrupt local traffic movement in this area.)

Median islands should be a minimum 10 feet in width, and provide for left turn pockets at major intersections. Medium-size trees with “accent” characteristics are recommended to complement frontage shade trees. The accent could be in the form of a notable habit, flower display, fall color, or other features. As recommended for frontage planting strips, drought tolerant groundcovers and/or low-shrubs are recommended to maintain visibility for motorists and pedestrians.

Community Center / Downtown Link

Installation of a more direct and attractive pedestrian route between the Community Center and Downtown is recommended. As illustrated by the “Community Center / Downtown Link” concept on the following page, this link would consist of number of relatively small, related improvements.

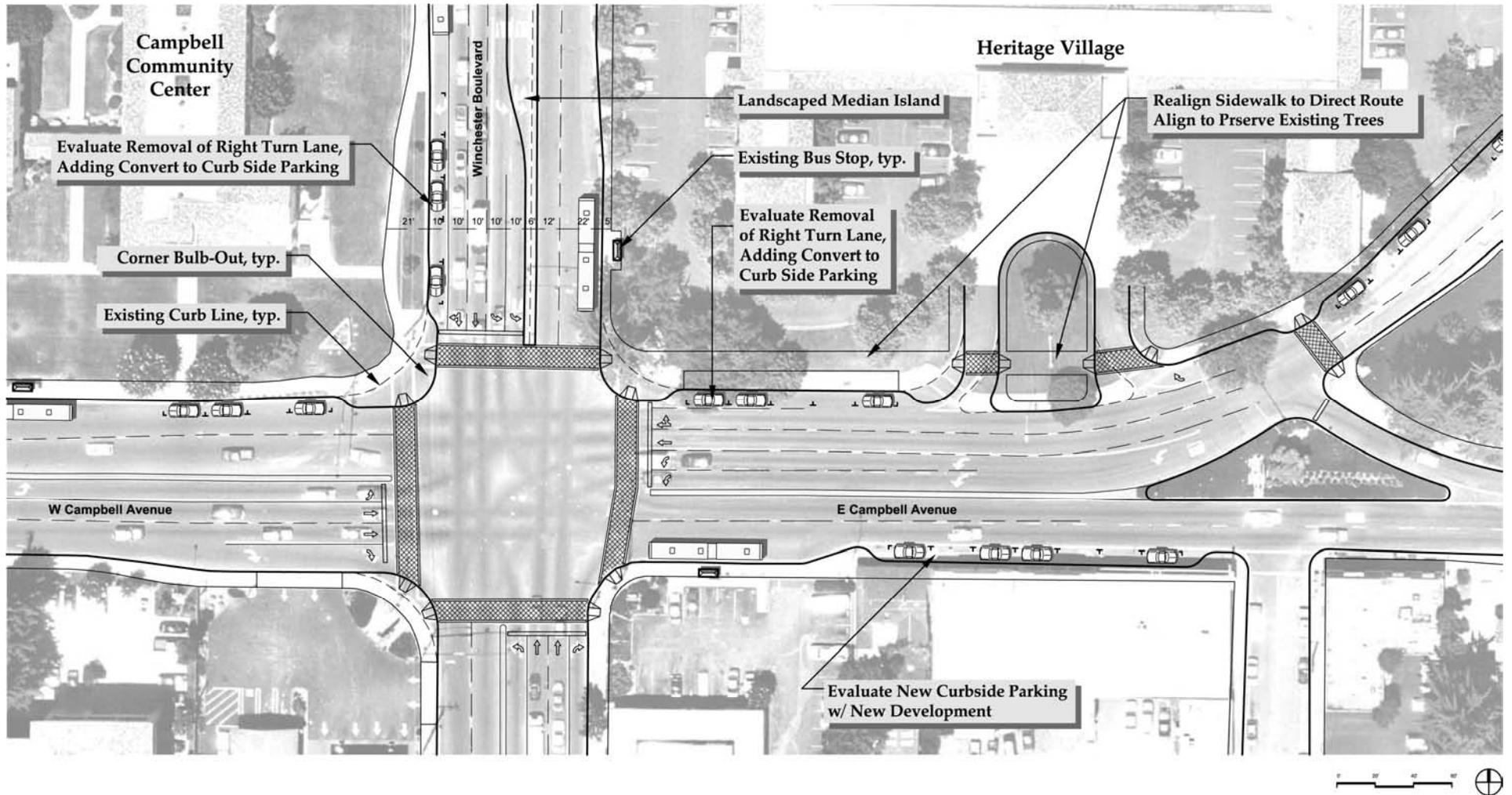
Large bulb-outs are recommended at the northeast and northwest corners of the Winchester/Campbell Avenue intersection. These would shorten the crossing distance significantly and reduce the speed of “free right” vehicle movements. They would also allow for creation of sheltered “parking pockets” along the westerly frontage of Winchester and the northerly frontage of East Campbell Avenue. Installation of these bulb-outs would require that existing dedicated southbound and westbound right-turn lanes be abandoned, however, and that adjacent through lanes be converted to through/right-turn lanes. Additional study by the City will be required to verify that these modifications will not result in significant degradation of the intersection level of service.

A new/improved crosswalk and pedestrian refuge is recommended for the north side of the intersection. This refuge could be accommodated by a combination of lane narrowing and/or

installation of a center median island. Installation of a full median island is not required; a small refuge could be installed with modification to adjacent lane widths.

The existing curbside sidewalk along the north side of East Campbell Avenue should be reconstructed or expanded to align with the new Winchester crosswalk. This sidewalk realignment should continue across the Heritage Village entrance drives and landscape island, with new paving, ramps, and crosswalks as needed. A new bulb-out and improved crosswalk is recommended for Civic Center Drive just east of the East Campbell Avenue “split.”

Improvements are recommended for the southerly portion of the Winchester/Campbell Avenue intersection as well. These include new sidewalk and street lighting, a bulb-out at the southwest corner (no lane modification required), and reconfiguration of the southerly frontage of East Campbell Avenue to accommodate curbside parking if new development occurs, similar to conditions within Downtown. This would require dedication of 7 to 8 feet of property along frontage to allow for this parking area and a sidewalk space.



Community Center / Downtown Link

VI. Plan Implementation

The objectives and provisions of this Master Plan shall not apply to any maintenance, repair, interior alterations and/or interior improvements to existing buildings within the Master Plan Area.

Where a structural addition or remodel is proposed to an existing building that exceeds the following thresholds, the plan will apply:

- 1) Projects that add 25 percent or more to the square footage of existing buildings on subject parcels shall comply with all provisions of the Master Plan;
- 2) Projects that do not meet the threshold set forth in paragraph (1) shall be required to comply with the objectives and provisions of this Master Plan as to only public improvement requirements if the project meets either of the following thresholds:
 - a) Projects that remodel more than 25 percent of the existing square footage.
 - b) Projects that alter more than 50 percent of the total building façade via a remodel.

For the purpose of remodels: Remodel includes any rebuilding or structural alterations that change support members of a building such as bearing walls, columns, beams, or girders that alter the exterior appearance of the building. It shall not include interior tenant improvements or structural alterations solely to meet code.

Exceptions:

- A. Upon application for an exception, the Planning Commission may hear and recommend that the City Council authorize a conditional exception to any of the requirements and regulations set forth in this plan; provided that the following facts are found:
 1. That there are special circumstances and conditions affecting subject property;

2. That the exception is necessary for the preservation and enjoyment of a substantial property right of the developer; and,
3. That the granting of the exception will not be detrimental to the public welfare, or injurious to other property in the area in which said property is situated.

B. Planning Commission recommendations:

1. In recommending such exceptions, the Planning Commission shall secure substantially the objectives of the regulations to which exceptions are requested, and shall act to protect the public health, safety, convenience and general welfare;
2. In recommending the authorization of any exception under the provisions of this section, the Planning Commission shall report to the City Council its findings with respect thereto, and all facts in connection therewith, and shall specifically and fully set forth the exceptions recommended and the conditions designated.

C. Upon receipt of such report, the City Council may approve the development plans with or without the exceptions and conditions recommended by the Planning Commission, and with other exceptions, if any, as the City Council deems necessary substantially to secure the purposes and objectives of this chapter.

Reconstruction after Damage or Destruction: A structure not meeting the standards of the Master Plan that is involuntarily damaged or partially destroyed by earthquake, fire, flood, wind, or other calamity or act of God or the public enemy, clearly beyond the control of the property owner may be reconstructed in compliance with the building code and without compliance with the Master Plan, only as follows:

1. Cost does not exceed seventy-five percent. A structure which is involuntarily damaged or partially destroyed to the extent that the cost of restoration does not exceed seventy-five percent of the cost of construction of a comparable new structure (as determined by the building official) may be restored or reconstructed; provided, the restoration is started within twelve months of the date of damage, and restoration is completed within twelve months thereafter;
2. Cost exceeds seventy-five percent. In the event the cost to repair the damage or destruction exceeds seventy-five percent of the cost of construction of a comparable new structure (as determined by the building official), the provision of this Master Plan shall apply.

Based on the parameters outlined above, public improvements contemplated in the Master Plan shall be implemented on an incremental basis as private development occurs. Undergrounding of frontage utilities is anticipated to be implemented through the establishment of an underground utility district.

Any public improvements required of private projects shall be reasonably related and roughly proportional to the nature and impacts of the project.

ACKNOWLEDGMENTS

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Jason Baker
Daniel Furtado
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