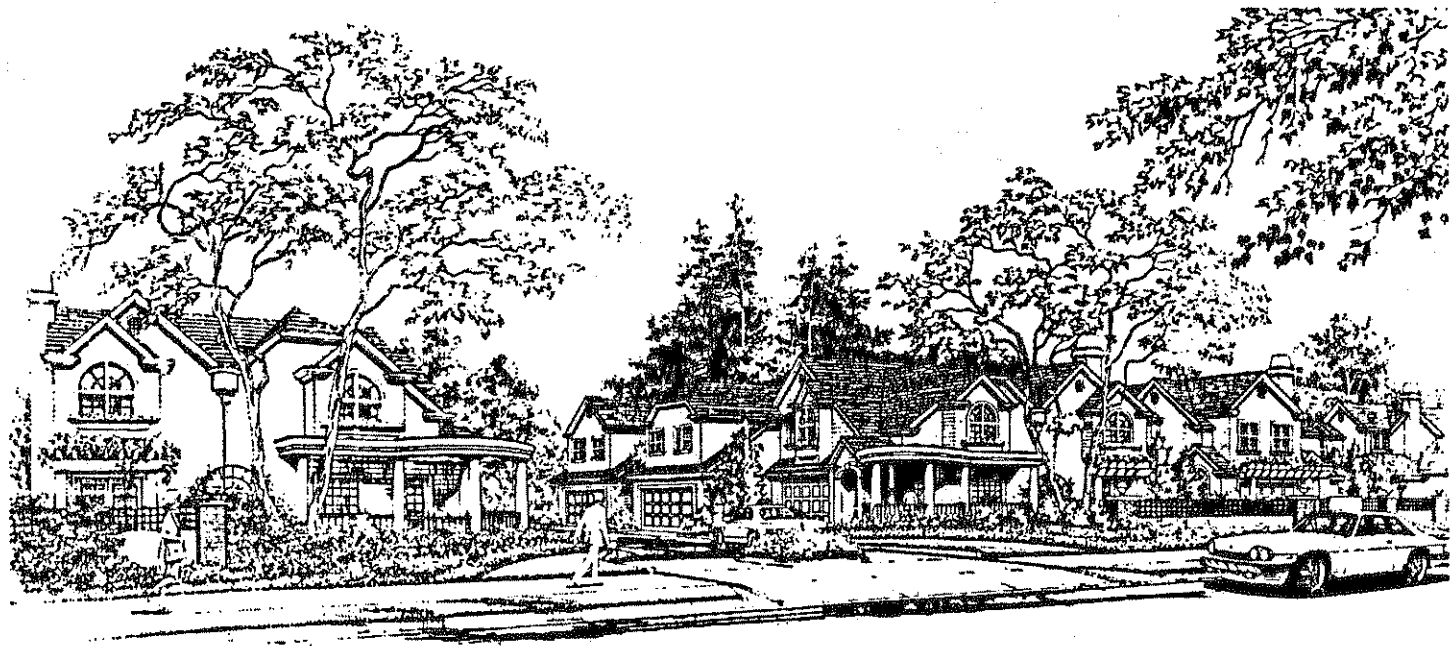
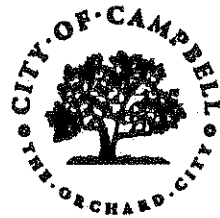


DESIGN GUIDELINES FOR LOW-MEDIUM DENSITY RESIDENTIAL PROJECTS

City of Campbell Community Development Department





**DESIGN GUIDELINES
FOR
LOW-MEDIUM DENSITY
RESIDENTIAL DEVELOPMENTS**

November 1994

PREPARED BY:

**CITY OF CAMPBELL
COMMUNITY DEVELOPMENT DEPARTMENT**

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INTRODUCTION

These guidelines were created to provide guidance to applicants and consistency in design review. The guidelines apply basic design principals which are general in nature and reflect the major concerns of site planning, architecture, and neighborhood compatibility.

These guidelines implement criteria for project review contained in the City's Site and Architectural Review Ordinance. As such, it serves as a basis to provide consistent design review from neighborhood to neighborhood across the City.

The guidelines are intended to assist property owners, architects, and developers in reaching successful solutions to their design needs. It also provides a framework for development which meets the City's goals and objectives for high quality development, and maintaining the positive qualities of Campbell neighborhoods.

Special or unique site conditions may necessitate variations from these guidelines. Variations will be considered when compensated by a related improvement which contributes to the excellence of the project. For example, deviation from the guidelines may be considered when compensated by an increase in open space or landscaping.

Applicants are encouraged to contact the Community Development Department prior to submitting an application to discuss the preliminary design of the project.

This will help the applicant determine if the proposal is consistent with design guidelines and zoning ordinance requirements.

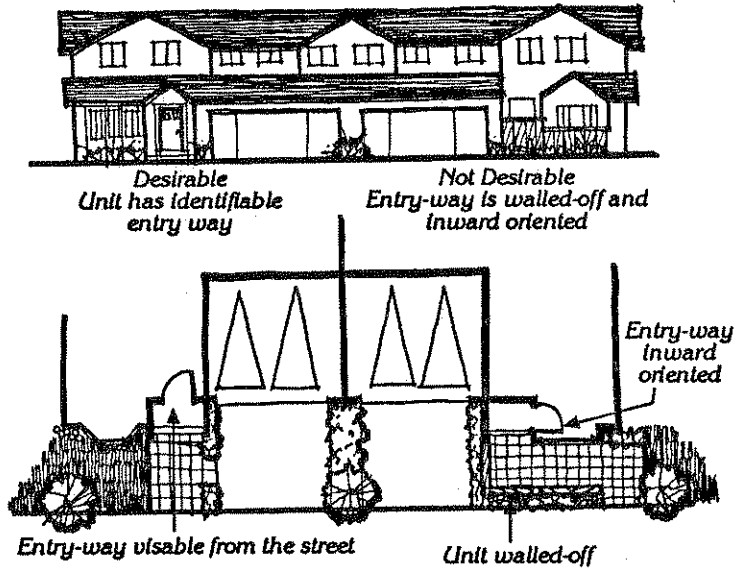
If you have any questions regarding the information in this guidebook, or about a specific project, please contact the Planning Division of the Community Development Department at 866-2140. The Community Development Department is located at City Hall, 70 North First Street.

A. SCALE AND MASS

1. The perceived scale and mass of the project should be compatible with developments in the surrounding area.
2. The use of architectural features, such as porches, that emphasize human scale and break up building mass are encouraged.

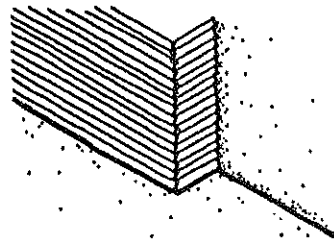
B. UNIT DESIGN

1. Units should have visible entry-ways. Units should not be oriented inward or walled-off by fences or garages.

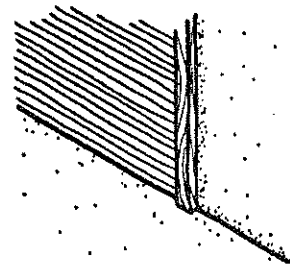


2. The entry-way of units adjacent to public streets should be oriented to the public street. The backs of units and privacy fences shall not face public streets.
3. Units should be setback from the common access drive a minimum of 10 feet.
 - a. A minimum of 10 feet of landscaping should be provided between the front living area of each unit and the street or common access drive, as measured from the edge of pavement or sidewalk (if present).
 - b. Porches and other similar architectural elements are encouraged and may encroach into the landscape area up to 5 ft.
 - c. Paths or walkways to the individual units may be placed in the landscape area but only if 50% or more of the area remains available for landscaping.
4. The architectural elements of buildings include building openings, doors, windows, columns, and dormers.
 - a. Architectural elements should be balanced on the building elevation. This can be accomplished by vertically and horizontally aligning the elements.
 - b. The type and style of architectural elements should be compatible with each other and the design of the unit.

5. Building materials shall be consistently applied and should be harmonious with adjacent materials. Changes in materials or colors should not occur on the same wall plane. Piecemeal and frequent changes in materials should be avoided.



Change of materials or colors should occur with changes in wall plane



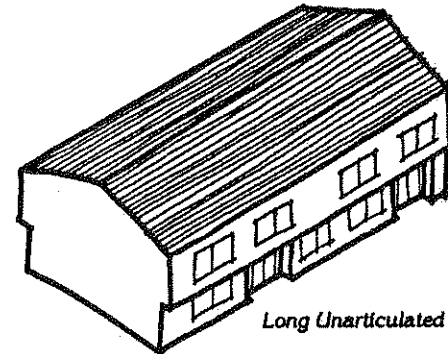
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C. BUILDING ARTICULATION

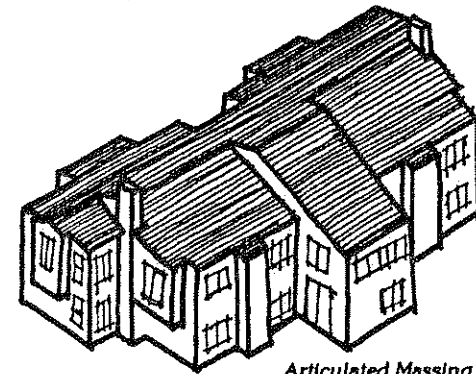
1. Design of units should avoid long unarticulated wall and roof planes. The intent is to avoid the barracks-like appearance of flat walls and roofs of excessive length.

This can be accomplished by staggering or offsetting wall planes or through the use of architectural elements such as balconies, arcades, porches, dormers, bay windows, and gables.

2. The width of the individual units should be expressed architecturally on the exterior elevation. This can be accomplished through the use of balconies, porches, and projections which articulate the individual units.



Long Unarticulated Massing

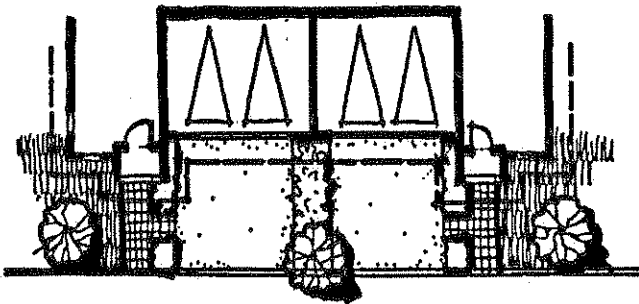
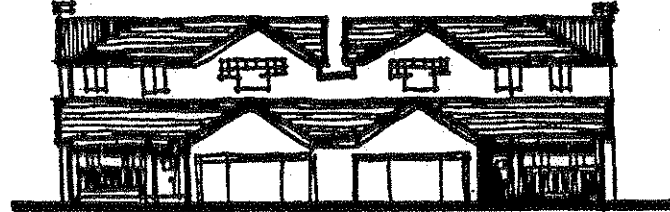


Articulated Massing

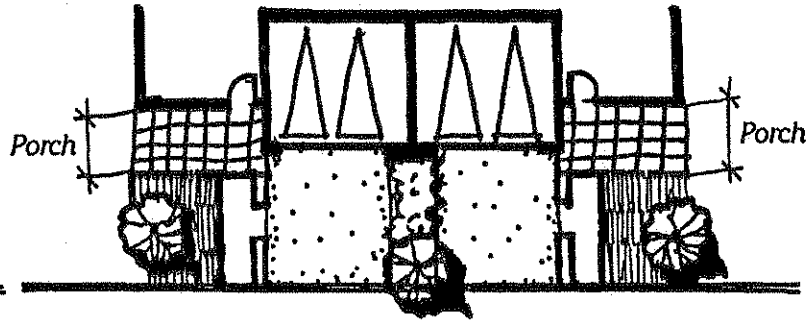
D. GARAGES

The living area of units should be the most prominent feature of the front facade. The appearance of garages should be minimized by incorporating the measures listed below, or other similar measures as approved by the Community Development Director:

1. Limit garage doors to no more than 50% of the linear front elevation of a unit or building group, with the remainder of the elevation devoted to living area or porch.



Recess the garage a minimum of 6ft.



Place a porch or other entry feature in front of the living area that extends in front of the face of the garage.

2. Garages which exceed 50% of the front elevation should either:
 - a. Provide an entry porch or other similar entry feature, in front of the living area, which extends beyond the face of the garage.
 - b. Recess the garage from the front wall of the living area a minimum of six feet.

E. OPEN SPACE

1. Projects should provide a minimum of 600 square feet of useable open space for every dwelling unit.
 - a. Of the required 600 square feet of open space, at least 300 square feet should be provided as useable private open space with the remainder devoted to common open space.
 - b. Private open space includes but is not limited to patios, balconies, and decks. However, not more than 30% of the open space requirement can be met by balconies or decks above grade.
 - c. Each unit's private open space area should have an average depth of 15 feet, with a minimum depth of 10 feet.
 - d. Useable private open space does not include: right-of-ways, vehicle parking areas, areas with a width and depth less than 10 feet, required setback areas, or common open space areas.
 - e. Private open space areas should be contiguous to the units they serve.
2. Common open space areas should be conveniently located for a majority of the units.
 - a. Projects greater than 10 units should provide 100 square feet of useable common open space for every unit.
 - b. Useable common open space should not include: right-of-ways, vehicle parking areas, areas adjacent to or between structures less than 15 feet apart, required setback areas, patios and private front, side or rear yards.
 - c. Common open space areas may include, but are not limited to, swimming pools, jacuzzi's and saunas, tennis courts, lawn areas, meeting and/or game rooms, and children's playgrounds.
 - d. Benches and shade trees should be provided in passive common open space areas.

F. LANDSCAPING

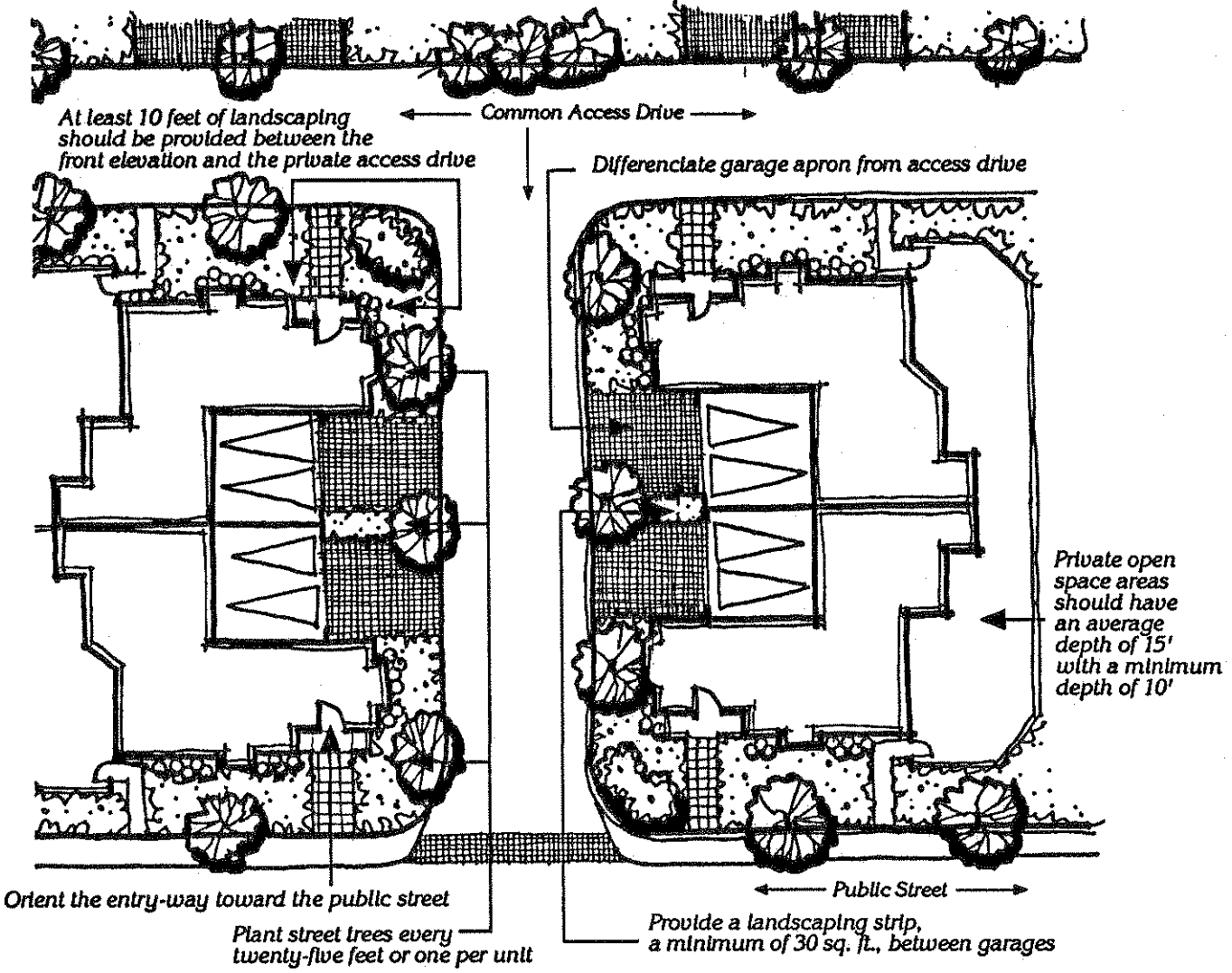
1. Landscaping/open spaces should be provided on at least 40% of the net lot area. This area includes the required landscaping in front of each unit, the ground floor of private open space areas, common open space areas, and the landscaping between garages.
2. New developments should preserve existing trees whenever possible per the Water Efficient Landscape Standards (WELLS).

G. COMMON ACCESS DRIVES

1. Primary common access drives should resemble a tree lined residential street with street trees planted approximately every 25 feet or one per unit. The intent is to soften building facades and the common access drive as viewed from the public street or once you enter the site.
 - a. Differentiated paving materials, textures, or colors should be used for garage aprons, the entry to the common access drive, and pedestrian walkways (when applicable).
 - b. Common access drives located behind units should be treated like a traditional alley with appropriate landscaping.

2. Enclosed garages that front on common access drives should provide a landscaping strip not less than 30 square feet between every two spaces.
3. Garage aprons should be at least 16 feet so that parked cars will not extend into the common access drives, or the garage aprons should be 10 feet so cars cannot use the apron for parking.
4. Landscaping should be used to screen open parking areas and unobstructed views of garage doors.
5. Projects should take into consideration pedestrian circulation from the public street to the individual units and internal circulation between units and common areas.
6. Lighting should be provided around the entire project including common open space areas.

PROTOTYPICAL SITE PLAN



H. PROJECTS ABUTTING SINGLE FAMILY HOMES

In addition to guidelines discussed above, the criteria below is intended to integrate townhouse projects abutting single family neighborhoods. Project design should be compatible with adjacent structures and fit within the context of the neighborhood.

1. To the extent possible, the public street elevation of any unit or building group should foster the appearance of single family residential design.
 - a. The width of the individual units should be expressed architecturally on the exterior elevation.
 - b. The project should incorporate distinctive architectural features found in surrounding developments such as window and door detailing, facade decoration, materials, roof style and pitch, entries, architectural style and porches.
2. The building design should contain traditional single family architectural elements. These elements may include, but are not limited to, defined entries, porches, projecting eaves and overhangs. The intent of this criteria is to provide an appropriate single-family residential scale and help reduce building mass.